

<p>Chair: John Ross Vice-Chair: Ryan Briese Commissioners: Tim Felton Justin Humenik Ruth Krueger Jason Pacht Stephanie Yendell</p>	 <p style="text-align: center;"><b>City of South St. Paul</b> <b>Planning Commission Agenda</b></p> <p style="text-align: center;"><b>Wednesday, September 7, 2016</b> <b>7:00 p.m.</b></p>	<p>City of South St. Paul 125 Third Avenue North South St. Paul, MN 55075 Phone: (651) 554-3217 Fax: (651) 554-3211 <a href="http://www.southstpaul.org">www.southstpaul.org</a></p>
<p>Hearing assistance PA system is available – if you need a hearing assistance unit please notify City staff before the meeting. <i>(If you use the hearing assistance PA system, please remove your hearing aid so it does not cause a feedback problem)</i></p>		
<p><b>Roll Call</b></p>		
<p><b>1. Agenda</b></p>		
<p><b>2. Minutes</b> A. August 3, 2016</p>		
<p><b>3. Public Hearings</b></p> <p>A. <u>Comprehensive Plan Amendment</u>: Changing the Future Land Use designation of a property in the community from Open Space to Industrial <i>(continued from 8/3/16 PC meeting)</i></p> <p>B. <u>Zoning Amendment</u>: Amendment to the North Riverfront Development District that would allow exterior storage as an Interim Use <i>(continued from 8/3/16 PC meeting)</i></p> <p>C. <u>Mad Further</u> (1725 Henry Ave.): Consider a request for a 3-year IUP to host a car show on the ramp of the South St. Paul Fleming Field Airport. The first event would take place on October 8, 2016 and subsequent car shows would be for mid-September in 2017 and 2018.</p> <p>D. <u>Danner, Inc. Proposal</u> (600 Verderosa Ave.): Consider a request for a 19,317 square foot building, with Conditional Use Permit for gas/diesel fueling station, exterior storage, variances for minimum building size, and an Interim Use Permit for a rock crushing operation.</p> <p>E. <u>Twin City Tanning Building Addition &amp; Site Plan Amendment</u> (501 Malden St.): Consider a request for a 6,254 square foot building addition to the southwest side of the existing facility and the addition of a parking lot at the NE corner of Clinton and Malden.</p> <p>F. <u>Zoning Code Amendment</u>: Amending Section 118-121 of the Zoning Code to allow up to two accessory structures to include one garage and one accessory structure <i>(continued from 7/6/16 and 8/3/16 PC meetings)</i></p>		
<p><b>4. New Business</b></p>		
<p><b>5. Other Business</b> A. Updates</p>		
<p><b>6. Adjournment</b></p>		

Next Planning Commission Meeting: October 5, 2016

This meeting is being taped by Town Square Television (NDC4): phone: 651-451-7834 web: [www.townsquare.tv](http://www.townsquare.tv)  
Replays can be viewed on Government Channel 19 on the Thursday following the meeting at 1:00 p.m. & 7:00 p.m.

**MINUTES OF MEETING  
SOUTH ST. PAUL PLANNING COMMISSION  
August 3, 2016**

MEETING CALLED TO ORDER BY COMMISSIONER JOHN ROSS AT 7:00 P.M.

Present:	John Ross	Absent:	Ryan Briese
	Jason Pacht		
	Ruth Krueger		
	Tim Felton		
	Justin Humenik		
	Stephanie Yendell		
	Peter Hellegers, City Planner		

- 1) APPROVAL OF AGENDA – Chair Ross noted item 4B. was withdrawn by the applicant – approved as presented – Pacht/Yendell (6-0)
- 2) APPROVAL OF MINUTES for July 6, 2016 – It was noted the motion to adjourn required correction – approved as amended – Krueger/Felton (6-0)

3) PUBLIC HEARINGS

A. Olson Garage Variance (1035 15<sup>th</sup> Avenue North): Consider an application by Jeremiah Olson requesting a variance to allow construction of a new garage that is 1.5 feet higher than allowed by City code.

Mr. Hellegers reported the applicant demolished a previous detached garage which was subject to flooding due to its low elevation. The homeowner is proposing a 24'x24' detached garage that includes a storage loft area totaling a height of 17.5'; however, the City code allows for a maximum garage height of 16' from the garage floor to the peak of the roof. The concrete slab already in place, the garage kit was ordered and trusses were delivered prior to the applicant applying for a building permit. Staff reviewed State Statute and City Code findings regarding practical difficulties used in connection with granting variances stating other remedies within the Code (i.e. building a 16' tall garage with a larger footprint or building an additional accessory structure to accommodate storage) were not considered by the applicant.

The applicant stated a considerable amount of money was expended for materials and was under impression approval had been granted. Additionally, the applicant stated the height variance is needed to provide for adequate storage and preserves much needed lot space for flowers and garden. He opined that constructing a larger garage was cost prohibitive and stated a taller garage is visually better than a storage shed.

A neighboring property owner submitted a letter in opposition and was in attendance to

state a nonconforming garage was removed and should not be replaced with another nonconforming structure. The applicant had the opportunity to build a garage that conforms to Code. The homeowner requested the Planning Commission follow City code and deny the request. The Commissioners were provided an additional letter in opposition to the variance.

Commissioner Krueger asked why the variance wasn't sought prior to purchasing materials stating the proper procedure wasn't followed with regard to applying for a building permit.

Commissioner Felton queried as to why the applicant thought approval was granted. Mr. Hellegers stated he couldn't speak to why that was the case as the garage building permit application didn't come in until after the concrete permit in May.

Commissioner Pachl stated an application must be submitted before approval is granted it appears the contractor dropped the ball and ordered materials before submitting the application. He opined granting a variance for additional loft space opens the door for future requests.

Chair Ross stated the variance request does not meet the threshold of practical difficulties adding that economic considerations do not constitute a practical difficulty. Other properties with large garages are not a factor, more space for storage is not a factor and fears a dangerous precedent on granting a variance simply to garner more lot space.

Motion to deny the garage height variance based on not meeting the practical difficulties threshold – Humenik/Yendell (6-0).

B. Twin City Hide Building Addition and Site Plan Amendment – withdrawn by applicant.

C. Concord Lanes, Inc. Variances (365 North Concord St.): An application for variances that would increase the gross allowable signage area from 150 s.f. to 520 s.f. for signage and allowed weathered wood exterior material for a portion of the east and south elevations of the building.

Mr. Hellegers stated with the transition from Wells Lanes to Mattie's Lanes the exterior wood siding materials and wall signage was removed which significantly reduced signage and visibility for the site. Due to the fact the business is setback at least 100' from the street the applicants are looking for signage variances to advertise the business. The zoning district is located on the cusp of the NCMU district just north of CGMU where sign standards are based on pedestrian level signage.

The proposed signage variance would only allow wall signage that is 10% of the visible wall area with the majority of signage facing Concord Street. The 10% threshold has been a standard the City has used for signage variance requests on large buildings. The NCMU district includes standards that signage is within a 10-15' tall band of the wall of the building; however, allowing a variance from the sign band allows for breaking up the longer blank wall spans of the building. The proposed exterior material of weathered wood plank

aids in breaking up the mass and providing more character for the building.

Chair Ross opined that the proposed exterior wood materials are an improvement to what is currently in place.

Commissioner Pachl queried as to whether the sign goes above the height of the structure. Mr. Hellegers reported the sign exceeds the building wall by 2 feet.

There was no one present to comment; however, staff received an email communication from a neighboring property owner concerned about illumination from signs that exceed the building height and Commissioner Pachl raised a concern about the lighting impacting the property owners on the bluff.

Staff responded that the wall signs with the exception of a south facing sign would be located on the east side of the building and would be backlit with one sign having halo lighting not casting light toward the bluff. It was staff's opinion that the styles of illumination (halo and internal) would not affect properties on the bluff.

Applicant Matt Anderson, Cecelia Dillion (project architect) and Jenny Zanatta (designer) of Shea, Inc. discussed exterior improvements go along with interior improvements being undertaken. Exterior improvements are needed to announce the vibrant atmosphere of the business. Additional signage provides increased visibility from the road, wayfinding to parking areas, entrances, etc. The outdoor seating area will remain; however, will be freshened with new furniture and fencing.

Commissioner Yendell stated it was a great design and would vote toward granting the variance.

Motion to approve the following variances:

- 367 square feet for the total amount of signage on the property
- signage to be located outside of the 10-15' tall sign band for the building
- allowing a weathered wood secondary exterior material

for 365 Concord Street North subject to conditions listed in the City Planner's report - Yendell/Felton (6-0).

D. Accessory Building Ordinance Amendments - Amending Section 118-121 of the Zoning Code to allow up to two accessory structures to include one garage and one accessory structure.

Chair Ross reported this item was continued from the July Planning Commission meeting. Mr. Hellegers reported the current Zoning Code states a property is limited to one garage (attached or detached) plus one other accessory building. The item was brought to the Planning Commission after the City Council directed staff amend the zoning code due to the difficulty encountered by a homeowner who wished to build a garage; however, they already had an existing garage and accessory structure on the property. The current Code would require removal of the accessory building (shed) prior to building the second garage.

Staff is proposing the following two alternatives:

Alternative A would amend the language to allow 2 accessory structures (detached garage and shed) when the property already has an attached garage. The 1,200 s.f. of allowable space would count against just the accessory (detached) buildings;

Alternative B would allow 2 accessory buildings (detached garage and shed) when there is an attached garage but the space for all accessory uses would be capped 1,200 square feet of space would be capped for all accessory uses (attached garage, detached garage, shed);

Discussion ensued regarding timing of the matter. Mr. Hellegers noted it is not necessary to make a decision by a specific date as nothing is pushing the 60-day rule. Commissioner Yendell commented there was also the option to change nothing as the previous variance request was a unique circumstance and didn't warrant changing the code.

Commissioner Felton stated Yendell's comment made sense and stated he didn't have an issue with holding the matter over until the next meeting.

Motion for continuance to the September Planning Commission meeting – Yendell/Pachl (6-0).

E. Comprehensive Plan Amendment: Changing the Future Land Use designation of a property in the community from Open Space to Industrial.

F. Zoning Amendment: Amendment to the North Riverfront Development District that would allow exterior storage as an Interim Use.

Items 3.E and 3.F were discussed together. Mr. Hellegers reported the City has received inquiries from two potential users interested in exterior storage on the City-owned property at the northeast corner of the city.

The Union Pacific Railroad is proposing to lease a 5-acre parcel on City-owned property for exterior storage of track materials for their railyard improvements and storage space for materials and rail cars. Mr. Hellegers explained the use would require an Interim Use Permit to allow the exterior storage. Additionally, the space is currently designated as Open Space which does not allow for exterior storage and would necessitate changing the Future Land Use (Comp. Plan) designation to Industrial. If the land use designation is changed to Industrial the zoning would require an amendment to allow exterior storage as a conditional use.

Chair Ross asked if anyone present wished to comment on the proposed Comprehensive Plan amendment for the NRDD area changing it from Open Space to Industrial or allowing exterior storage in the NRDD.

The following residents spoke in opposition to amending the Comprehensive Plan for the NRDD area changing it from Open Space to Industrial:

Dennis Walter (1581 N. Concord) stated the NRDD was put in as a buffer from Barge Channel Road in St. Paul and believes the Union Pacific Railroad would seek additional railroad tracks if the amendment was approved. The City made an investment in ballparks and going from Open Space to Industrial is a drastic change and doesn't go with the trail. He also had concern that industrial is not the right image for the area. The area is the gateway to the City and doesn't believe it's a right fit.

Sara and Ben Reno (1654 Willis) expressed concern over their river view, the effect on the wildlife and what the DNR has to say regarding the matter.

Staff reported the DNR and MRCCA would be contacted for their comments. In addition, adjacent communities will be notified in advance of the September meeting.

Mike Doggs (1503 Willis) indicated there is already noise from the airport, railroad including diesel fumes from trains. He is in favor of leaving the area as green space for a legacy for children and grandchildren. Has an environmental impact study been considered?

Stan Krueger (1315 Kassan Ct.) stated the area is subject to flooding and the storage would need to be moved easily in the event of flooding. In his opinion an industrial use should not be adjacent to the new ballfields. Railroad tracks are not temporary.

Richard Steffels (1514 Willis) – 30-year resident stated the value of his property will decrease if the view of the river goes away and opined that if the property is developed the deer will go away.

Joe Moen (1504 Willis) stated he purchased the property four months ago; however, had he known this would take place he wouldn't have made the purchase.

Chair Ross thanked the residents in attendance for their patience in waiting for the discussion of the agenda item and requested staff to place the items at the top of the September meeting agenda.

Commissioner Felton thanked the residents for their opinions and expressed reservations stating the matter will be an uphill battle.

Motion to continue the matters to the September 7, 2016 Planning Commission meeting including holding the public hearing portion open – Ross/Humenik (6-0)

#### 4) NEW BUSINESS

A. Proposed Ordinance on Temporary Family Health Care Dwelling Units: Consider an ordinance opting out of State Statute, Section 462.3593 Requiring Cities to Permit Temporary Family Health Care Dwelling Units.

Mr. Hellegers reported the State of Minnesota recently approved legislation regarding Temporary Family Health Care Dwelling Units that requires cities to either allow the structures relative to the terms of the legislation or to adopt an ordinance opting out of the legislation by September 1, 2016. The approved legislation would allow temporary dwelling

units (less than 300 square feet) to be brought onto a property that has an existing home.

Mr. Hellegers explained the units are limited to six months on the site with a one 6-month renewal period. Staff stated the following concerns: permitting the temporary health care dwelling units takes away the zoning authority of the city, required permit information causing HIPPA concerns, the \$100 permit fee and \$50 renewal doesn't cover the city's cost of administration/inspection. Group homes and assisted living facilities are already in place for those needing assistance. Staff recommends approving PC Resolution No. 2016-6 opting out of the requirements of Minnesota Statutes, Section 462.3593.

Discussion ensued.

Commission Krueger and Felton raised concerns that the health issue may not be resolved after the six month or one-year term and didn't feel the temporary dwelling unit was the appropriate way to deal with the issue.

Commissioner Yendell commented that South St. Paul prides itself on being family friendly, that there would be limited risk to the City, the number of people that would qualify would be limited to families in crisis and doesn't believe allowing the structures would cause additional onus on the city.

Commissioner Pachl stated financially it doesn't make sense and he isn't in favor of the legislation due to the fact there are other viable options.

Motion to deny Planning Commission Resolution No. 2016-06 recommending approval of the ordinance to opt out of the State requirements for Temporary Family Health Care Dwellings - Yendell/Humenik (2-4) - Motion failed.

Motion to approve Resolution No. 2016-06 recommending approval of the ordinance to opt out of the State requirements for Temporary Family Health Care Dwelling – Felton/Krueger (4-2) – Motion passed.

Motion to adjourn – Yendell/Felton (6-0).

Planning Commission Meeting Date:  <b>Wednesday, September 7, 2016</b>	 <b>City of South St. Paul Planning Commission Report</b>	PC Agenda Items:  <b>3.A and 3.B</b>
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<b>Project Name:</b>	Comprehensive Plan Amendment – NRDD area
<b>Request:</b>	Consider an amendment to the Comprehensive Plan to change the future land use designation from Open Space to Industrial. Consider a zoning amendment to allow exterior storage as an interim use in the NRDD zoning district.
<b>Proceeding:</b>	<ul style="list-style-type: none"> <li>▪ Discussion</li> <li>▪ Planning Commission Resolution (Recommendation to City Council)</li> </ul>
<b>City Council Meeting Date(s):</b>	City Council – 1 <sup>st</sup> Reading – September 19, 2016 (tentative) City Council – 2 <sup>nd</sup> Reading – October 3, 2016 (tentative)
<b>Exhibits:</b>	<b><u>Item 3A and 3B:</u></b> A. PC Resolution 2016-07 – Comprehensive Plan Amendment B. PC Resolution 2016-08 – Zoning Amendment C. Correspondence received from resident(s) D. Maps (zoning)

**Update**

These items were continued from the August 3<sup>rd</sup> Planning Commission Meeting. Notice was sent to adjacent communities regarding the proposed amendment and we hope to either have comments from all of these communities or a waiver of comments by the time of the meeting.

An email from a resident was also received since the last meeting and has been included as part of this packet.

**Background**

The City owns a 7-acre parcel at the northeastern corner of the community. In recent years the City has had some development interest from neighboring properties such as the neighboring Saint Paul impound lot and United Pacific Railroad (UPRR), both of which would want the site for exterior storage. Due to the industrial nature of surrounding properties and the fact that these are currently the only properties with ways to directly

access the property it makes sense that these are the parties that have expressed interest. The property is currently guided as Open Space in the future land use for the most recent Comprehensive Plan and this designation would not allow exterior storage nor would it allow other development options other than park and recreation space, trail, or marina. The City owned parcel is inland from the river and adjacent to several industrial properties to the north in Saint Paul so if a comprehensive plan amendment is to be considered that would be the most consistent designation. Additionally, if the City was to act on either of these proposals the City would also need to amend the zoning to allow exterior storage as a conditional or interim use.

What motivated this current discussion about the comprehensive plan amendment and zoning change is interest from the UPRR to lease a 5-acre portion of the City owned parcel for exterior storage that would be used to store track materials to be used in their railyard improvements and expansion that would occur within their existing rail right of way. Additionally, this property would provide storage space for materials and rail cars after the improvements have completed. If the City were to entertain this concept it would also require the UPRR to go through a separate Interim Use Permit approval process to allow the proposed storage use of the property. The Interim Use Permit would be the time to consider the specifics of that use but for the present discussion on the Comprehensive Plan Amendment and Zoning Amendment it is important to think about the idea of industrial and exterior storage as a general land use.

## **Property Characteristics**

### **Future Land Uses**

The property is part of a 7-acre parcel owned by the City that is located at the northern border with Saint Paul. The property to the north is in the City of Saint Paul, the property to the east is guided Open Space, the property to the south is guided Park, and to the west is the railroad. Concord Street is west of the railroad with no other development on the right side of the road and residential property on the west side of Concord.

### **Existing Land Uses**

The property to the east is the Alter industrial property, to the south is the Kaposia Landing park, to the west is railroad, and to the north is the Saint Paul impound lot.

### **Zoning**

The property is currently zoned North Riverfront Development District (NRDD) and has a Public Land overlay. Permitted Uses in the NRDD include boat marinas and public recreation uses; Conditional Uses in NRDD would allow commercial recreation.

### **Access**

The property is currently landlocked with no street access to the property. This is likely the main reason the property has not already been developed.

### **Other considerations**

The Mississippi River Regional Trail (MRRT) has a proposed extension that would run from Kaposia Landing park and through the far western portion of the property, wind

through Saint Paul and would eventually connect to Harriet Island Park in Saint Paul. This MRRT extension project has received grant approval which would be funded and construction to begin in the spring of 2017. Some of the potential conflicts with the proposed trail and proposed UPRR storage use by include the ensuring sufficient buffer space around the trail so the trail user experience isn't substantially diminished and a proposed at grade crossing of the trail to provide access into the site.

### **Item 3E:**

#### **Request**

Consider a Comprehensive Plan Amendment to change the future land use designation of a property from Open Space to Industrial.

### **Item 3F:**

#### **Request**

Consider a Zoning Amendment to allow exterior storage as an interim use for the NRDD zoning district.

The property is part of a 7-acre parcel owned by the City that is located at the northern border with Saint Paul. The property to the north is in the City of Saint Paul, the property to the east is guided Open Space, the property to the south is guided Park, and to the west is the railroad. Concord Street is west of the railroad with no other development on the right side of the road and residential property on the west side of Concord.

#### **Process / Timeline**

Process – The Planning Commission will continue a discussion on the item (held over from the August 3<sup>rd</sup> meeting), make a recommendation, and the Comprehensive Plan Amendment and Zoning amendment would be forwarded to the City Council for consideration.

#### Steps:

1. Hold public hearing on comprehensive plan – *if still waiting for comments continue to October 5<sup>th</sup> PC meeting*
2. Hold public hearing on zoning amendment – *if still waiting for comments continue to October 5<sup>th</sup> PC meeting*

#### **Staff Analysis**

The interest in the property raises the question about whether the property is guided correctly and whether there are other potential uses for the property. The likely uses would derive from the surrounding property types and a limiting factor is the lack of access to the property. Some recreational uses may be a harder fit due to the lack of access and the nature of the surrounding industrial properties and railroad which could diminish the quality of a recreational use. Additionally the City is already developing an 87 acre park just south of the site. Therefore open space/passive recreation use on the parcel would

likely be best served as providing adequate natural buffers around the trail to preserve the user experience. The next most likely development option would be an industrial land use given the nature of the development in Saint Paul and the Alter metal recycling use directly east of the site. It's possible that the site could accommodate a more desirable industrial use than exterior storage but the City would need to provide access to the site and would need to obtain an access easement to provide that access. Additional analysis of the site would also be necessary to determine condition of the soils and other issues impacting develop potential. If the that analysis determines that the site is limited then exterior storage or open space may be the highest and best uses of the property.

### **Staff Recommendation**

Staff recommends a Comprehensive Plan Amendment to allow for industrial and open space uses with industrial focused to the interior portion of the property and the open space use reserved to buffer the future MRRT trail extension. Amending the comprehensive plan allows the City to consider development options for the property though does not lock it in to any one development scenario. Ultimately since the City owns this property it also has that control over whatever may be developed on that site. Staff does not recommend the zoning amendment at this time. However if exterior storage of the site were to be entertained staff would recommend that it be limited to the northern and eastern portions of the site so that the use would not impact the proposed trail along the western edge of the property.

Respectfully Submitted,

*Peter Hellegers*

Peter Hellegers, City Planner

City of South St. Paul  
Dakota County, Minnesota

**PLANNING COMMISSION  
RESOLUTION NUMBER 2016-07**

**RECOMMENDING APPROVAL/DENIAL OF A COMPREHENSIVE  
PLAN AMENDMENT TO CHANGE THE FUTURE LAND USE FROM  
OPEN SPACE TO INDUSTRIAL**

**WHEREAS**, the City's Comprehensive Plan guides future land uses for property within the City, and

**WHEREAS**, the subject property is located at the northeastern corner of the community and is shown as Open Space on the Future Land Use map from the Comprehensive Plan, and

**WHEREAS**, the subject property is currently undeveloped and is surrounded by existing Industrial uses and railroad lines, and

**WHEREAS**, in order to guide the property to its highest and best use and make the property consistent with neighboring land use the future land use would need to be guided as Industrial instead of Open Space, and

**WHEREAS**, an extension of the Mississippi River Regional Trail (MRRT) is proposed along the western side of the property which would connect the existing trail system to the south with Harriet Island in Saint Paul to the north, and

**WHEREAS**, future land uses of property to the east whether Industrial or Open Space can coexist with the proposed MRRT trail extension, and

**WHEREAS**, the Planning Commission opened a public hearing on the proposed resolution at their August 3, 2016 meeting and continued the public hearing to their September 7, 2016 meeting; and

**NOW THEREFORE, BE IT RESOLVED** by the Planning Commission of the City of South St. Paul, Minnesota, as follows:

1. Approval/Denial of An Amendment to the Comprehensive Plan to change the Future Land Use from Open Space to Industrial.

Adopted this 7<sup>th</sup> day of September, 2016.

\_\_\_\_\_  
Chair

ATTEST:

\_\_\_\_\_  
City Planner

City of South St. Paul  
Dakota County, Minnesota

**PLANNING COMMISSION  
RESOLUTION NUMBER 2016-08**

**RECOMMENDING APPROVAL/DENIAL OF A ZONING AMENDMENT  
TO ALLOW EXTERIOR STORAGE AS AN INTERIM USE IN THE  
NORTH RIVERFRONT DEVELOPMENT DISTRICT**

**WHEREAS**, the subject property is located at the northeastern corner of the community in the North Riverfront Development District (NRDD), and

**WHEREAS**, the purpose of the NRDD is to promote and regulate the development of mixed commercial and certain high-density uses in the area adjacent to and in the vicinity of the Mississippi River by the balancing of utilization of land for the purposes for which it is most appropriate and protection of the river amenities in the public interest, and

**WHEREAS**, the is currently undeveloped and is surrounded by existing Industrial uses to the north and east and railroad lines to the south and west and does not currently have street access to the property, and

**WHEREAS**, the City of South St. Paul (“the City”) has an interest in amending the zoning regulations in the NRDD district to include Exterior Storage as an Interim Use, and

**WHEREAS**, the Planning Commission opened a public hearing on the proposed resolution at their August 3, 2016 meeting and continued the public hearing to their September 7, 2016 meeting; and

**NOW THEREFORE, BE IT RESOLVED** by the Planning Commission of the City of South St. Paul, Minnesota, as follows:

1. Approval/Denial of an Amendment to Section 118-130 to allow Exterior Storage as an Interim Use in the NRDD Zoning District.

Adopted this 7<sup>th</sup> day of September, 2016.

\_\_\_\_\_  
Chair

ATTEST:

\_\_\_\_\_  
City Planner

## Peter Hellegers

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**From:** Lois Glewwe <lglewwe@hotmail.com>  
**Sent:** Wednesday, August 24, 2016 9:18 AM  
**To:** Peter Hellegers  
**Subject:** Change to SSP Comprehensive Plan

Dear Mr. Hellegers and Members of the Planning Commission:

I am unable to attend the September 7 Commission Meeting where you will hear public comment on the proposed change to South St. Paul's Comprehensive Plan to accommodate the request of the railroad to expand into property zoned green space on the north end of South St. Paul adjacent to the Regional Trail. I have a fractured spine and am unable to get around at the moment, but I hope you will take my comments into consideration.

Back in 1989-1992 I was Chair of the Chamber of Commerce Railroad Expansion Task Force and a member of the SSP City Council. At that time the Chicago Northwestern Railroad sought permission to create the largest switching yard in the Twin Cities on their property along the Mississippi River in South St. Paul. The community, after weeks of town meetings, public hearings and petitions, successfully mounted a campaign to deny the request even though the case ended up in court. Neighbors on the bluff above the river were already experiencing loss of property values and moving out because of the noise and damage and smells the railroad brought to their backyards. Windows were broken, shelves inside homes gave way and the area was becoming undesirable because of the presence of the railroads. The eagles, who were newly returned to the flyway in South St. Paul were endangered as was all other wildlife.

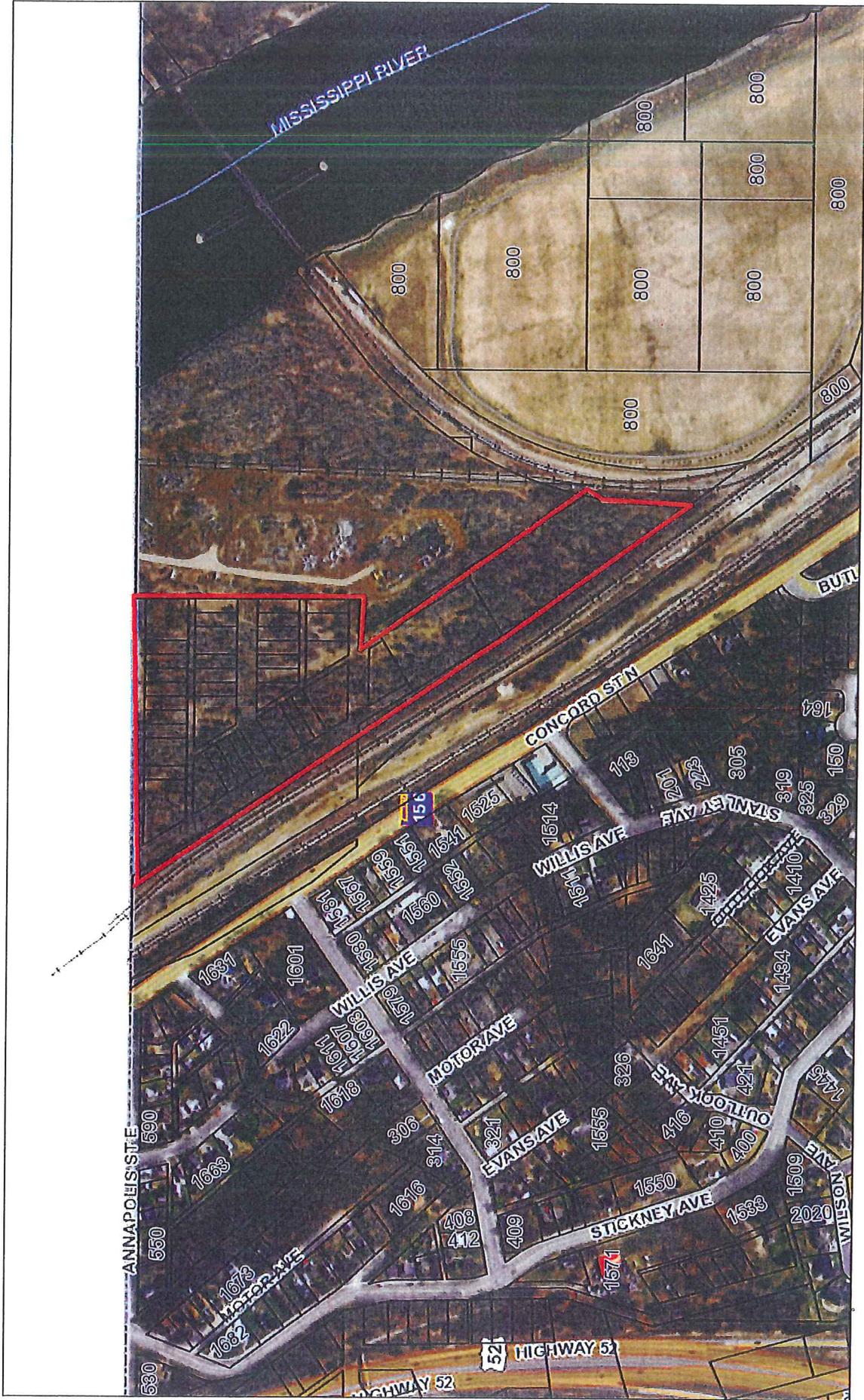
Since then, we negotiated with the railroad for access to the trail; the railroad improved their buildings and although they continued to expand the number of tracks in South St. Paul, there were several years when both noise and pollution were somewhat abated. Now, under the current railroad, I have watched with extreme concern as the buffer of trees which helped shield the entrance to South St. Paul from the north have been removed, revealing the hundreds of oil tankers being stored in our city, the significant track expansion and the encroachment onto zoned green space for exterior storage of tracks and equipment. The railroad has donated money to the Youth Task Force, entertained the Chamber of Commerce and pretended to be good community citizens even as they continue to seek to take over and destroy what little green space we have left.

There is often very little cities can do to protect themselves from railroad expansions and pollution dangers but in this case we still have the power to deny this current request and I ask you all to do that and protect our citizens, our wildlife and our city from further degradation from the railroad.

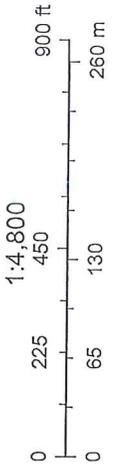
Thank you,

Lois Glewwe  
1514 Waterloo Avenue  
South St. Paul, MN 55075  
651-457-3403

# Dakota County, MN

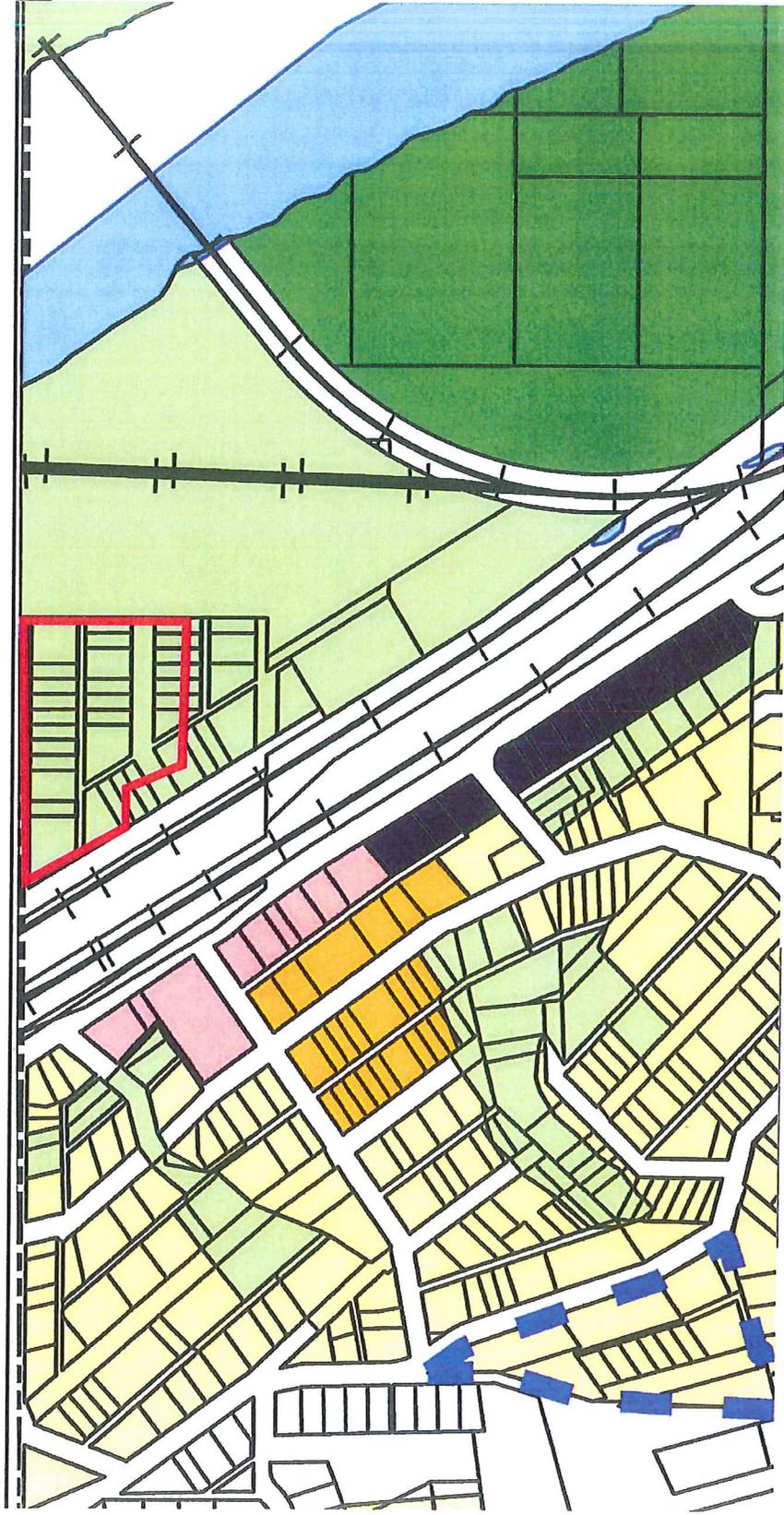


August 2, 2016

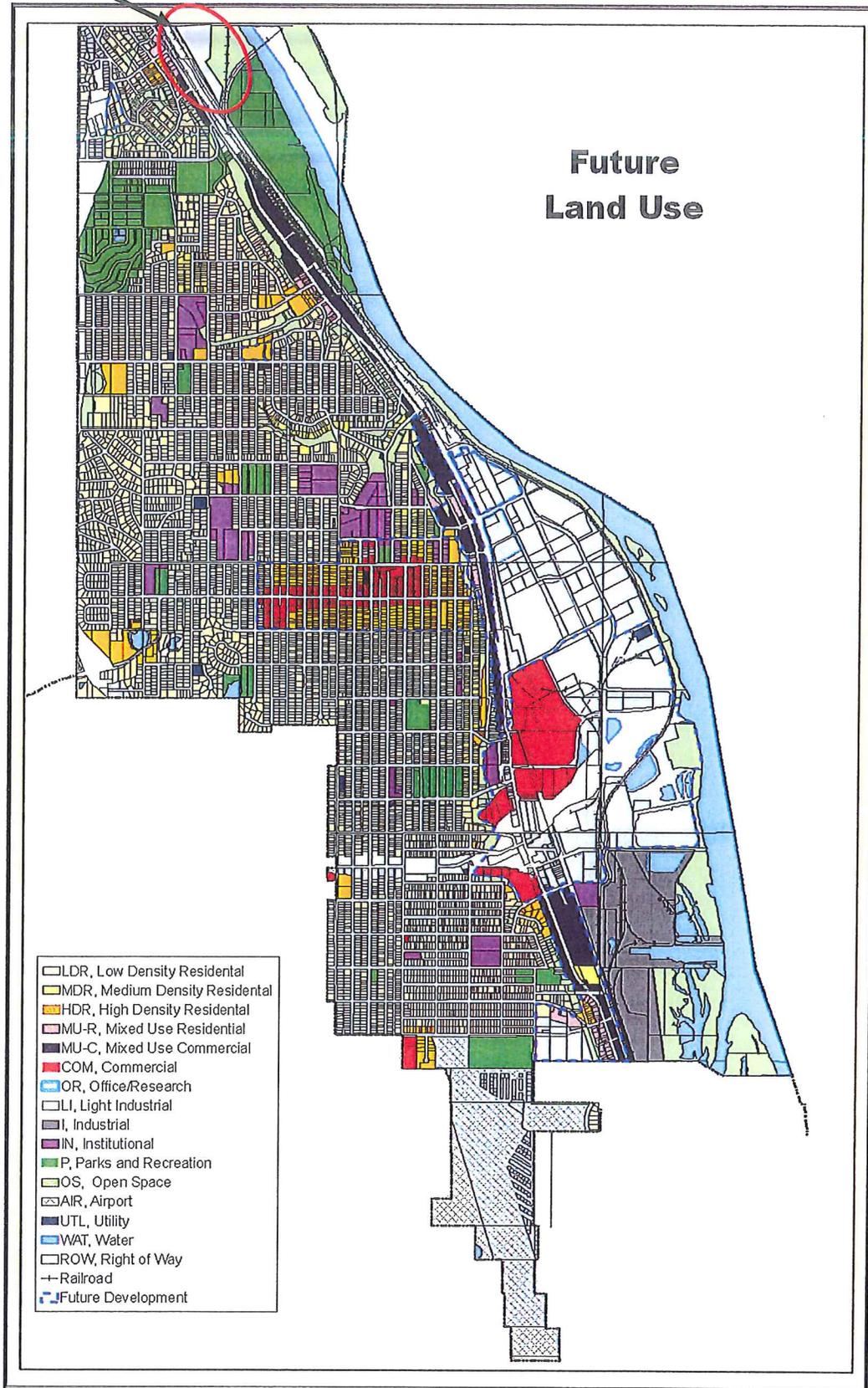


Disclaimer: Map and parcel data are believed to be accurate, but accuracy is not guaranteed. This is not a legal document and should not be substituted for a title search, appraisal, survey, or for zoning verification.

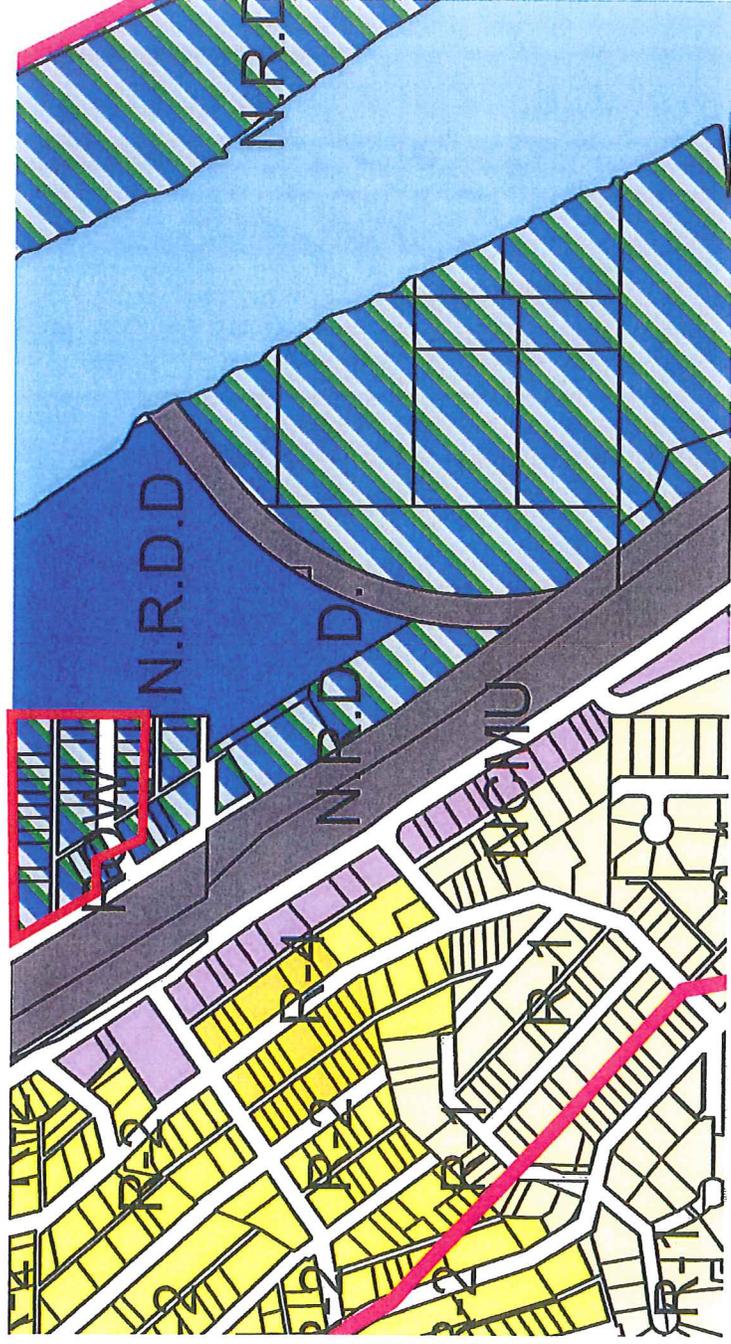
Site in Context – Future Land Use (Comp Plan)



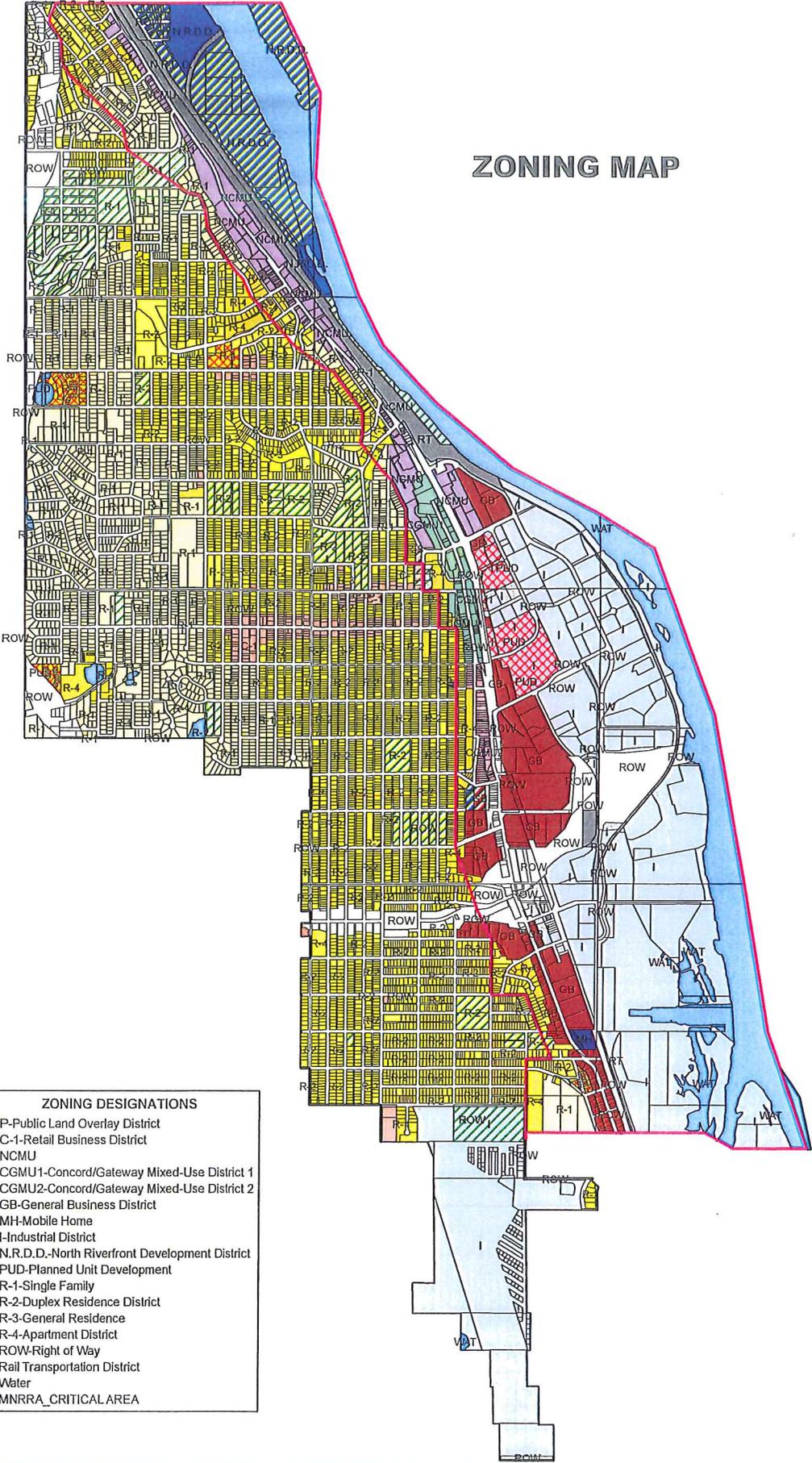
NRDD Area – proposed change



# Site in Context - Zoning



# ZONING MAP

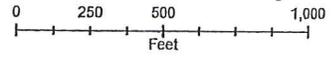


**ZONING DESIGNATIONS**

- P-Public Land Overlay District
- C-1-Retail Business District
- NCMU
- CGMU1-Concord/Gateway Mixed-Use District 1
- CGMU2-Concord/Gateway Mixed-Use District 2
- GB-General Business District
- MH-Mobile Home
- I-Industrial District
- N.R.D.D.-North Riverfront Development District
- PUD-Planned Unit Development
- R-1-Single Family
- R-2-Duplex Residence District
- R-3-General Residence
- R-4-Apartment District
- ROW-Right of Way
- Rail Transportation District
- Water
- MNRRRA\_CRITICAL AREA

**Metropolitan  
Airports Commission**

**St. Paul - South St. Paul  
Regional Trail Gap**



- Existing Regional Trail/Greenway
- Concept Regional Trail/Greenway

**St. Paul  
Port Authority**

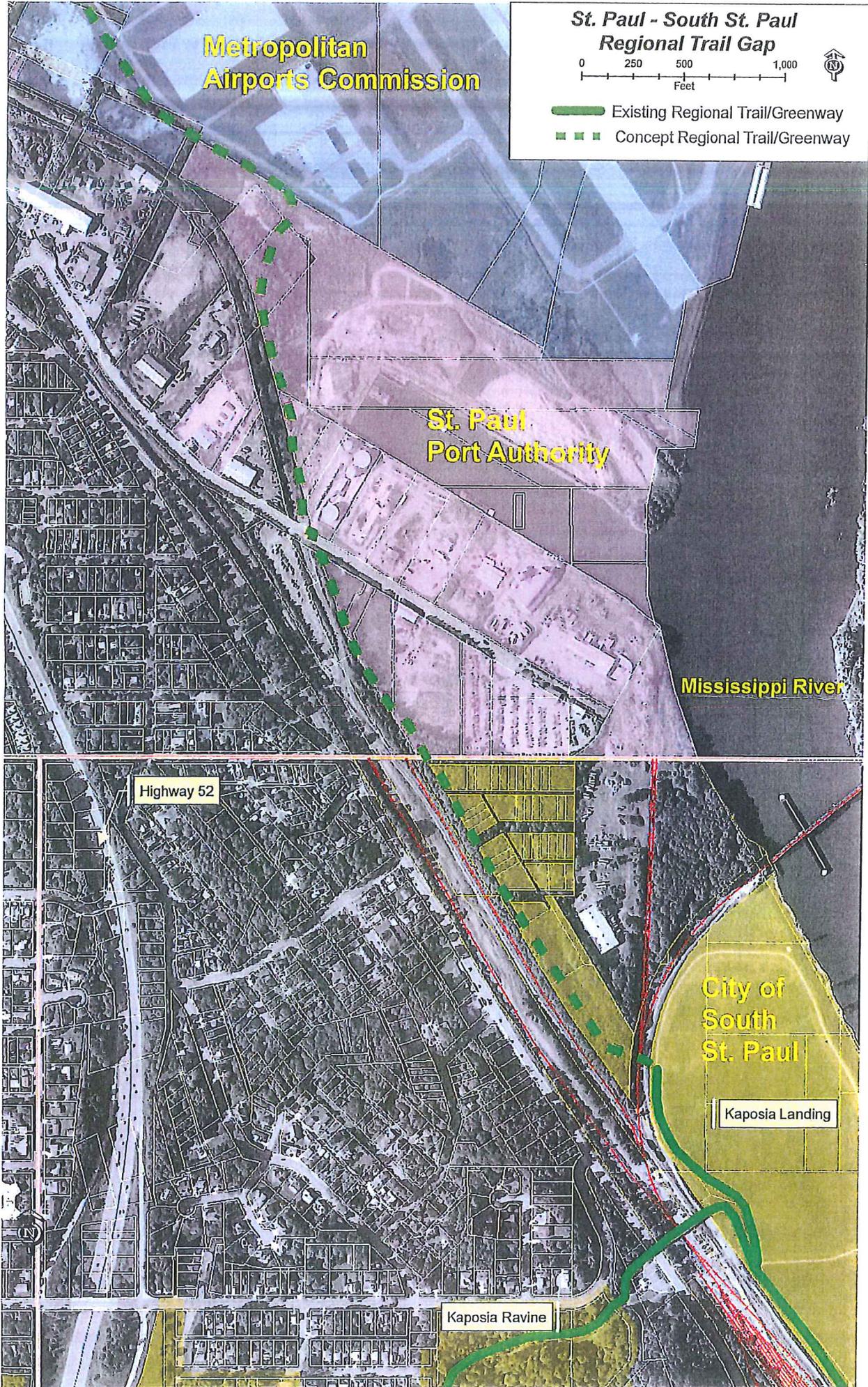
**Mississippi River**

**Highway 52**

**City of  
South  
St. Paul**

**Kaposia Landing**

**Kaposia Ravine**





Planning Commission Meeting Date:  <b>Wednesday, September 7, 2016</b>	 <b>City of South St. Paul Planning Commission Report</b>	PC Agenda Item:  <b>3.C</b>
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<b>Project Name:</b>	<b>MadFurther Car Show IUP – Fleming Field Airport</b>	
<b>Site Address:</b>	1725 Henry Avenue	<b>PID:</b> 361113000011
<b>Applicant:</b>	MadFurther, LLC	
<b>Request:</b>	Consider the request for an IUP with a 3-year term to have a car show on the ramp at the Fleming Field Airport on October 8, 2016, another on September 16, 2017 and the last show on this term would be September 15, 2018.	
<b>Proceeding:</b>	Public Hearing / Planning Commission Recommendation	
<b>Tentative City Council Meeting Date:</b>	September 19, 2016	
<b>Deadline:</b>	<b>October 8, 2016</b> <small>*The City must act on this request by this 60-day review period deadline unless the city provides the applicant with written reasons for why additional time, up to a maximum of 120 days, is required. The City may extend the review period beyond the 120 days but only with the applicant's consent.</small>	
<b>Exhibits:</b>	A. Location map B. <del>Correspondence from neighboring property owners</del> – (none) C. Materials submitted by the applicant	

**BACKGROUND**

The MadFurther car show is the idea of Further Performance a local European car specialist. The proposal is to utilize the ramp (south and east of the terminal building at 1725 Henry Avenue for the car show and the applicant would coordinate with the CAF on the use of portable toilets, security and similar arrangements as the CAF is hosting the beer garden. The MadFurther event would run from 10 am to 5 pm, with additional time for set-up before the event and clean-up after the event as may be arranged with the Airport Manager. In addition to the cars at the car show the applicant is proposing to have 3 food trucks, 30 or fewer vendors, and a DJ. The DJ would be set-up inside the CAF building and the speakers would be set so they are not directed toward nearby residences. Vendors would be for things like cars parts and accessories and would be set up by 9 am and done by 6 pm. The food trucks would be set up by 10 am and would be done by 5 pm. The applicant will be responsible for making sure their event is cleaned up. The display area is approximately 400 feet by 250 feet. Guest parking is 148 spaces and vendor parking is 28 spaces. This will be the 4<sup>th</sup> time the auto show has been held at the airport and staff has not received complaints about the use.

**EVALUATION OF THE REQUEST**

A. INTERIM USE PERMIT

Following are standards from the City's Zoning Code that apply to specifically to Interim Use Permits.

1. Interim Use Permit Findings

The applicant is seeking an Interim Use Permit (IUP) for their annual car show which would run from approximately 10 am to 5 pm on Saturday, October 8, 2016 and would follow a similar schedule for September of 2017 and September of 2018. The Interim Use Permit would terminate at the end of September 2018. City Code Section 118-41, states that the City Council may grant an interim use permit for the interim use of property, if the following criteria can be met:

Interim Use Permit Criteria:

- ✓ *The use conforms to the zoning regulations.*
- ✓ *The date or event that will terminate the use can be identified with certainty.*
- ✓ *Permits for the use will not impose additional costs on the City, if it is deemed necessary for the City to take the property in the future; and.*
- ✓ *The user agrees in writing to any conditions that the City Council deems appropriate for permission of the interim use.*

City staff has determined that the proposed interim use would meet the criteria. First, the proposed use as a special event would be allowed in the Industrial zoning district and the airport has hosted several such events. Second, the use would be one weekend day in October or September for each of the next three years with the IUP terminating on September 30, 2018. Third, the permit would not impose additional costs should it be necessary to acquire the property, as the City already owns the property. Finally, the conditions are listed in staff's recommendation and would be incorporated into the resolution to approve the Interim Use Permit.

**ALTERNATIVES**

**The Planning Commission has the following actions available on the proposed application:**

A. **Approval.** If the Planning Commission finds the application to be acceptable, the following action should be recommended for approval:

- Approval of the **Interim Use Permit** for a car show at the South St. Paul Fleming Field Airport subject to the following conditions:

1) **Compliance with Plans/Submittals.** The site shall be utilized in substantial conformance, in the reasonable opinion of the City Council, with the application, narratives, and with the following plans on file with the Community Development Department:

- |   |                  |
|---|------------------|
| a) Application/Narrative/Proposal (MadFurther)          | dated 08/09/2016 |
| b) Event Site Map (MadFurther / City of South St. Paul) | dated 07/16/2013 |

2) **Lease Required.** The applicant shall be required to have a lease with the City of South St. Paul for the use of the property.

- 3) **Sales/Display Limited to the Designated Area.** No selling or display shall occur outside of the designated areas.
- 4) **Applicant Responsible for Clean-up.** The applicant shall be responsible for all clean-up activities.
- 5) **License Required for Food Vendors.** The food vendors must obtain the necessary licenses from the Minnesota Department of Health (MDH) before the applicant and/or food vendor business can begin conducting business in the city. The food vendors must also keep the MDH license in good standing.
- 6) **Noise.** The applicant and event DJ, vendors, exhibitors, and food vendors shall comply with all City regulations regarding noise levels and shall make reasonable efforts to minimize loud noises that may impact the nearby residential area. Speakers and noise emitting equipment shall be directed away from residential properties.
- 7) **Compliance with Laws and Approvals.** The property must remain compliant with all federal, state, and local laws and ordinances and all prior City approvals.
- 8) **Term of the Interim Use Permit.** The Interim Use Permit shall allow for one annual car show and the term of the Interim Use Permit shall be for three (3) years beginning on October 8, 2016 and terminating on September 30, 2018. The applicant will be allowed to host one car show for each of these three years on a weekend day in October or September with hours that run from 10 am through 5 pm. Additional time for set-up before the event and clean-up after the event is subject to the approval of the Airport Manager. The final dates for the car show in 2017 and 2018 are subject to the approval of the Airport Manager.
- 9) **Termination of the Interim Use Permit.** The violation of a condition of approval shall terminate the Interim Use Permit.

B. **Denial.** If the Planning Commission does not favor the proposed application or portions thereof, the above requested should be recommended for denial. If the Planning Commission recommends denial, then findings of the basis for denial should be given.

#### **STAFF ANALYSIS**

Staff has reviewed the proposal and believes that the proposed 1-day event can reasonably be administered through the IUP process. The applicant has shown that they will take measures to minimize the potential for nuisances on nearby residential properties.

#### **STAFF RECOMMENDATION**

Staff recommends **approval** of the proposed car show Interim Use Permit, subject to the conditions listed in this report.

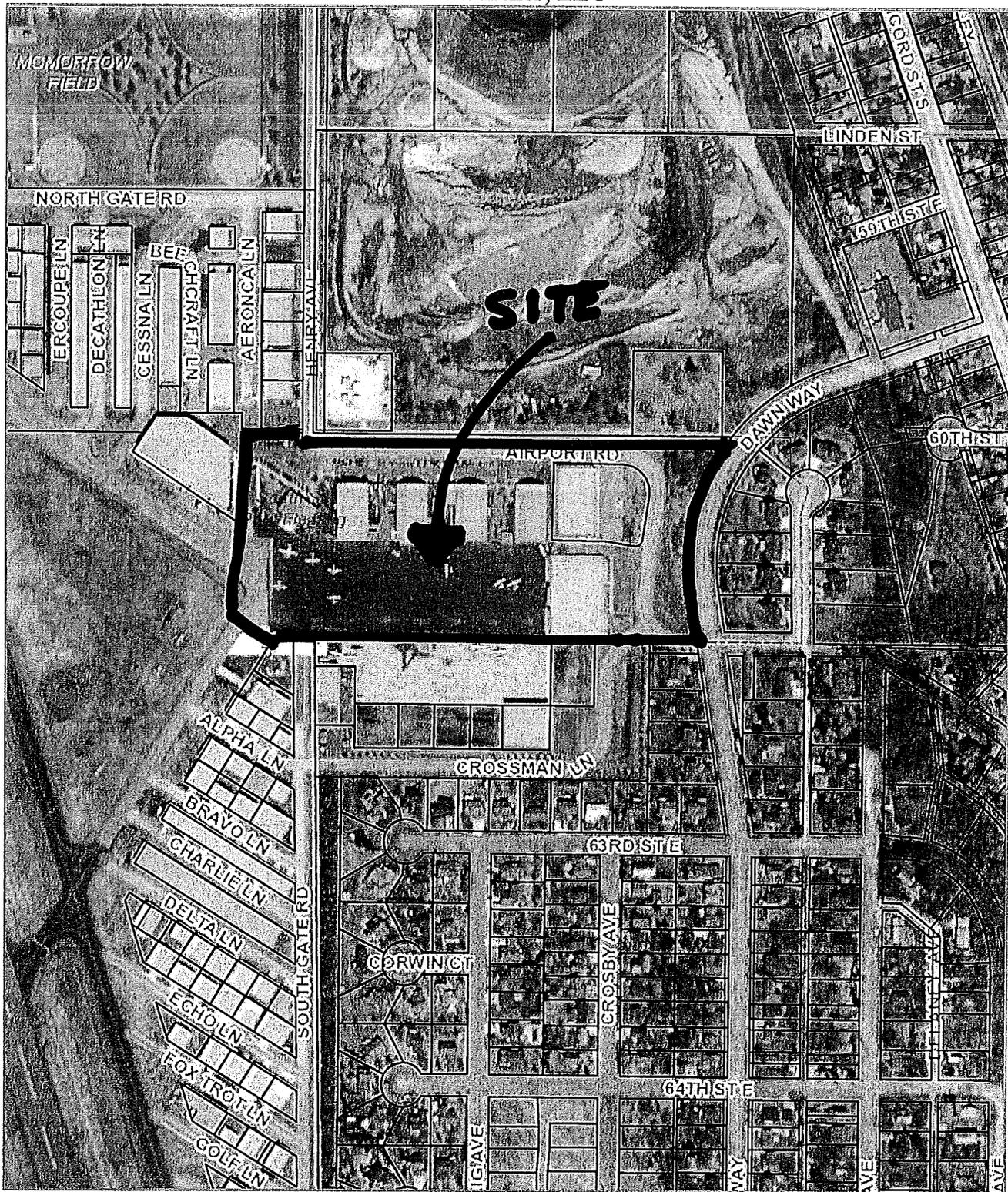
Respectfully Submitted,

*Peter Hellegers*

Peter Hellegers, Planning Division Manager

A.

Mad Further, LLC



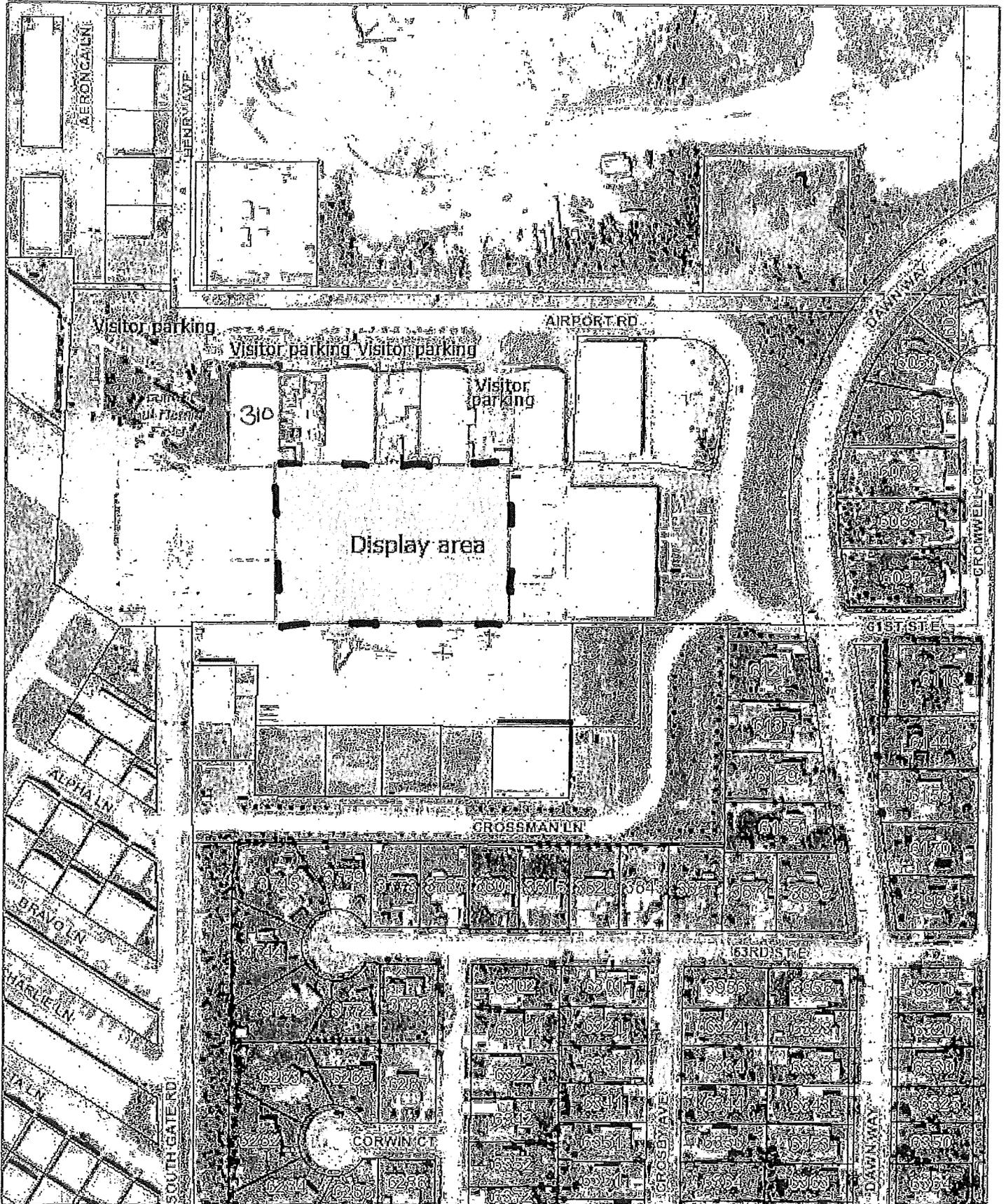
Disclaimer: Map and parcel data are believed to be accurate, but accuracy is not guaranteed. This is not a legal document and should not be substituted for a title search, appraisal, survey, or for zoning verification.

Map Scale  
 1 inch = 400 feet  
 8/18/2016

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Ag

# 310 AIRPORT RD/SSP AIRPORT



Disclaimer: Map and parcel data are believed to be accurate, but accuracy is not guaranteed. This is not a legal document and should not be substituted for a title search, appraisal, survey, or for zoning verification. Dakota County assumes no legal responsibility for the information contained in this data.

Map Scale  
1 inch = 226 feet

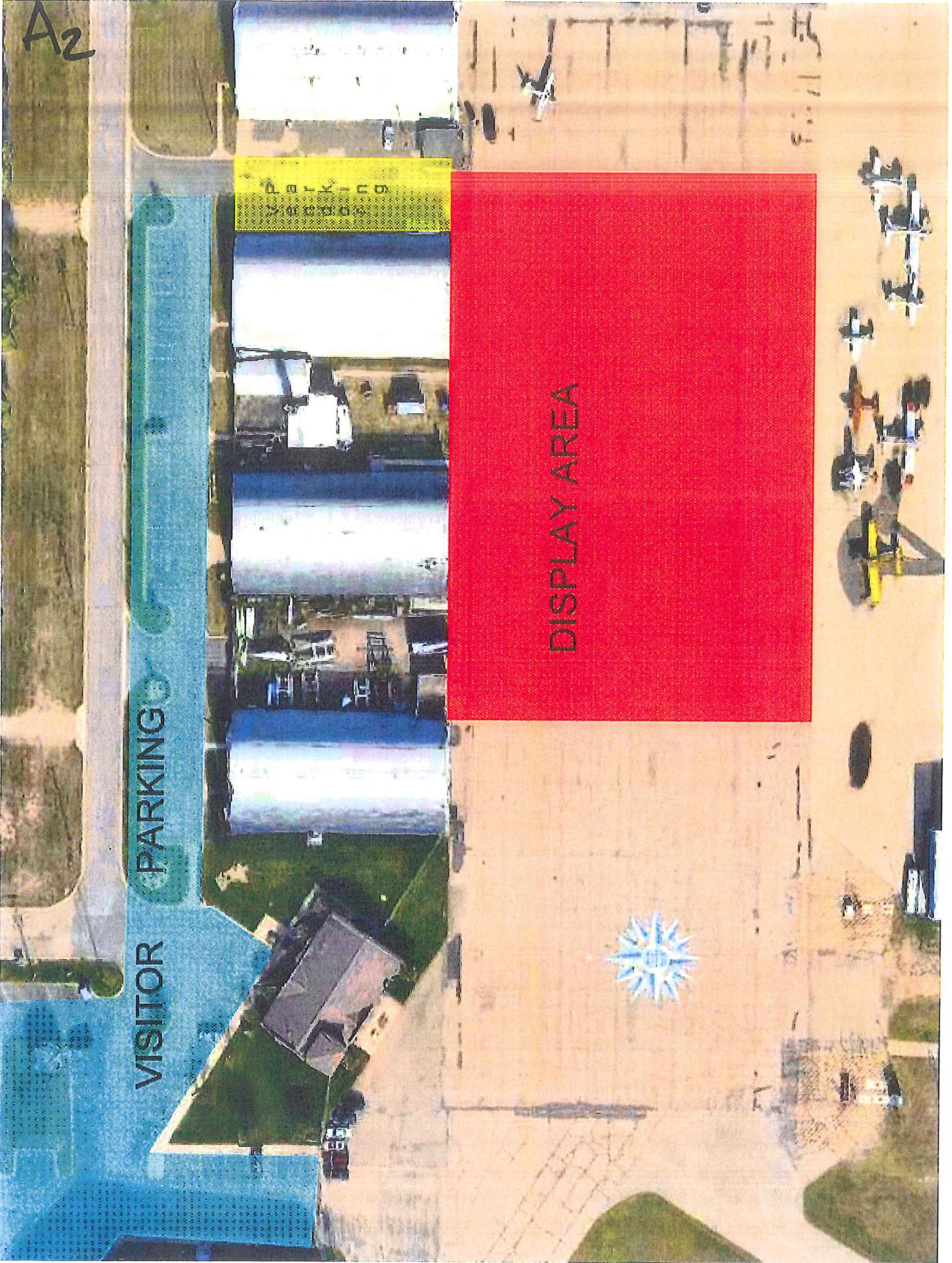
A2

VISITOR

PARKING

DELUXE CO  
WOODS

DISPLAY AREA





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## **Introduction:**

Further Performance is a local European automotive specialist with operations in Minneapolis since 2005, and was the organization behind the "*MadFurther*" automobile and airplane show that was held on September 7, 2013 at the Fleming Field's CAF Hangar and aircraft deck in South Saint Paul.

*MadFurther* aims to become an annual automobile show and gathering that brings together aircraft and automobile enthusiasts and owners of all marques. It is a community-building event that brings together individuals who identify themselves as enthusiasts, owners, preservers, restorers, and admirers of old and contemporary vehicles alike, and the art and technology that is associated with them. The show is a unique opportunity for attendees of all ages to observe rare and custom vehicles from close proximity, and also to interact with the individuals who build, modify, and maintain them. The inaugural event in 2013 brought together several hundred attendees on a single day and met overwhelming success based on the feedback that the Further Performance staff received. The inaugural show comprised of an airplane show, that included short flights with members of the public on-board, a car show with prizes on several categories, and a thematic dance as the closing festivity in the evening.

After the initial success and with the experience gained from the inaugural event, the Further Performance team is presently planning the 2nd annual *MadFurther* event.

Our mission is to give local Midwest enthusiasts their own car show, highlighting various car enthusiast cultures from the region. We plan to have a variety of car genres and marques in the show, including European, Domestic, Japanese Domestic Market, Hot Rods, and motorcycles.

Very often folks have to drive in excess of ten hours to attend a car show and display their specific cars. This is an inspiring aspect of the automobile culture for the team at Further Performance. We're interested in continuing to build an event that includes communities of car enthusiasts from all backgrounds. We have built a community around our shop, and around this show, and we would like to have an interesting event to showcase vehicles and to network with each other.

The proposal for MadFurther 4 is structured as follows. Several small sections provide details on the following aspects of organization:

- Location and timeline
- Partnerships / Sponsorships
- Advertising
- Pre-registration and online registration
- Parking facilities
- Washrooms and waste management plan
- Food / drink services and vendors
- Entertainment and noise ordinance
- Safety / Evacuation plan
- Operations and control
- Setup / Clean-up
- Contact information

### **Location:**

We are proposing to use the same location as the inaugural event, the Fleming Field's CAF Hangar in and aircraft deck. Based on the experienced gained during the inaugural event, we have determined that the venue is ideal for hosting this type of events. The event location (venue) is proximal to Minneapolis, it is easy to access and provides a great backdrop for the *MadFurther* event. It should be noted that there are non-functional aspects of the venue that make it particularly attractive. More specifically, the vintage military vehicles found on the premises are an excellent complement to the automobiles, and tie-in well with our love of all things mechanical.

The area on the tarmac between the old airplane hangars and the taxiway will be used for displaying the aircraft and automobiles. The aircraft will be positioned closer to the taxiway while automobiles will be positioned in rows with ample space between them

### **Partnership:**

We have partnered with the Twin Cities chapter of the Commemorative Air Force (CAF), which is located at Fleming Field. The CAF has agreed to allow us to use their hanger. The CAF have offered their fleet of Military vehicles for display during the show.

With this partnership we are hoping to bring some insight and awareness to the Commemorative Air Force and what they do, as well as the airport facility itself. This partnership will help with our vending needs and the CAF's income needs.

More specifically, the CAF will be providing all public beverages during the show (see below). Experience from last year has shown that this partnership contributed to having hosted a high quality family-friendly event.

We have also partnered with two local food-trucks, which were in attendance at the inaugural event in 2013. We will also be expanding the food vendor line-up with an additional truck; this is to be determined and depends on scheduling and availability.

### **Sponsorship:**

Further Performance is the main sponsor and creator of the *MadFurther* car show.

We may solicit outside title sponsorship, and will be working with other vendors in the automotive industry to provide limited on-site vendor space. Ideally we would like at least one vendor that specializes in each vehicle category.

### **Advertising:**

Further Performance will be advertising for the *MadFurther* car show via social media (Facebook, Twitter, Instagram), websites, word-of-mouth, and fliers that will be distributed at other car shows (EuroHangar (Michigan), Dubs In The Valley (Wisconsin)). We may also use print media for limited advertising, but we will be mostly making use of digital marketing strategies. We expect roughly 1200 to 2000 spectators and 300 to 500 vehicles to attend this year. Last year's attendance was nearly 1200 persons and over 200 vehicles.

The Commemorative Air Force will be advertising for the event as well, via social media (Facebook) and their local contacts.

### **Registration:**

Anyone who would like to enter his or her vehicle in the show will have to fill a registration form. We will have pre-registration online shortly, and we will only close it after the show opens. Participants will also be able to register day-of-show.

There will be a registration fee, which will help fund and support the event, covering property and facility rental fees, event insurance, awards, labor, and advertising costs.

### **Parking:**

Upon arrival, we will require the registered show cars to check in at the gate entrance. At the station, each vehicle's class and registration information will be verified and then will be directed by a staff member to the appropriate parking area.

We will have numerous staff and volunteers that will help to ensure the show cars are parked in the correct areas. We will work closely with the Commemorative Air Force to make sure we are following Fleming Fields ramp parking guidelines for spectators.

We will also provide spectator parking and signage dictating its location; this will be located outside of the ramp area. We will have staff / volunteers directing traffic to ensure spectators are parked in the correct areas and that there are no traffic delays.

### **Food Service:**

We plan to invite local food trucks to our event this year as last year food supply and access was sufficient. However, we are anticipating a larger crowd for the 2016 event. Since we are collaborating with the Commemorative Air Force, food trucks can set up near the hangar so they can have electrical access if needed. The food trucks will provide food only, while the CAF will provide all of the beverages.

### **Washroom and Waste Facilities:**

We will have numerous restroom facilities located near the hangar. Trash bins will be positioned throughout the ramp, parking, staging area and vendor areas. We are working with the Commemorative Air Force to ensure that we have adequate restroom, trash bins, and recycling bins for the show.

## **Entertainment and Noise Ordinance:**

We will hire a team of DJs for this event. We plan to have an area set up near the middle of the tarmac for the DJ booth and the *MadFurther* information and first aid booth. We will work with the Commemorative Air Force and Fleming Field staff concerning City Ordinances regarding noise and speaker placement. We plan to change make a change on the South-facing four speaker setup. We will be using 4 to 6 speakers surrounding the show perimeter and aimed towards the middle. This is for two reasons. First, to allow for better sound reception on the show, and second, to direct sound away from the nearby residential area. We plan to use a reputable event management company, Henley Audio of Minneapolis, which handled the inaugural event, for sound setup and staffing the audio management booth.

## **SETUP / CLEAN UP:**

We would like to start setting up the Friday evening before the car show if possible. We would also like to have access to the ramp and hangar at 8AM the day of the car show. We'll start registration at 9AM, and the show, including audio, will begin at 10AM.

We will ask vendors to retract signage, clean their vendor area, and leave by 6PM. We will also ask the public and show-cars to leave by 5PM. The Further Performance volunteers will also help the Commemorative Air Force with tear down and clean up after the show. All clean up should be completed by 8PM.

## **SAFETY PLAN:**

### **COMMUNICATIONS**

Event coordinators will communicate with event volunteers in person before the event, and during setup as well as during the event if needed. We will discuss our safety plan and layout prior to the start of the event. Event coordinators will be available by citizen-band radio and mobile phones throughout the event.

Announcements and signs will be posted throughout the event pertaining to the schedule and safety information. Announcements will also be made via the sound system.

## **COMMAND AND CONTROL:**

Available for contact via cell phone and radio (during the event):

*Owner/Operator/Chief Manager/Producer:*

Tristan Henderson | 612-296-6524 | Tristan@fptuned.com

*Project and Production Manager:*

Mike Olson | 612-203-8288 | Mike@fptuned.com

*Further Performance Staff:*

Frank Buntzen | 612-812-6505 | rottenkrout@gmail.com

Matthew Henderson | 781-626-0560 | motorhead567@comcast.net

*Producer/Event and Production Consultant:*

Marnie Gamble | 612-867-1913 | marnie@suemclean.com

## **RESPONSIBILITY OF INDIVIDUAL AGENCIES/GROUPS**

### ***Parking/Traffic Control:***

#### Mike Olson

Mr. Mike Olson will supervise 9 to 12 individuals who will direct vehicles on the ramp and make sure they are parked in the correct areas. Volunteer staff will be identifiable by a MadFurther logo shirt. They will communicate via radio.

### ***Ramp/Aircraft Safety:***

#### Frank Buntzen

Mr. Frank Buntzen will be on staff to ensure attendees behave in a safe manner around the aircraft. We will also have a Fleming Field staff member and members of the Commemorative Air Force on hand to make sure aircraft is secure and attendees behave in a safe manner.

### ***Hangar Safety/Crowd Control:***

#### Matthew Henderson

Mr. Matthew Henderson along with a Fleming Field staff member and members of the Commemorative Air Force will ensure that crowd order is maintained within the designated show field and hangar area. We will have two MadFurther staff members available as assistants.

### ***All Volunteers:***

All volunteers will be briefed on standard information (restroom locations, emergency exits, fire extinguishers, etc.) and will be identifiable by either a MadFurther logo shirt or a Commemorative Air Force logo shirt. The volunteers will have a printed note-card with a map, information and specific instructions.

All volunteers will also be briefed on how and who to contact in case of an incident / emergency. All volunteers will assist in preventing unauthorized access to secured areas, and will actively work to keep any noise to a minimum.

## **SAFETY RESOURCES**

- EMT on site at the MadFurther/ First Aid/ DJ booth
- Fire extinguishers
- Hand held/portable radios
- Cell phones
- First Aid kits
- Security Barriers
- Signs
- Portable restrooms

## **LOST/FOUND PERSONS**

In the event of lost and found persons, volunteers will be directed to notify event coordinators. Coordinators will assign a hold location and make an announcement over the PA system.

## **LOST/FOUND PROPERTY**

We will have a lost and found bin stationed in a main area of the event. If items are not claimed during the event, the items will be held at the Commemorative Air Force to be picked up on Wednesdays or Saturdays.

## **EVACUTATION PLAN**

- Event coordinators will make the decision to evacuate. Police officers will be consulted/called.
- Event coordinators will be in charge and will brief/call the staff via cell phones or radios.
- Event coordinators will be responsible to make sure all attendees have exited the building. Safety office and ramp safety will direct people to evacuation area.
- Public will be directed to the posted emergency exits.

- For a fire, the public will be evacuated to the grass area west of the hangar.
- For severe weather emergency, the public will be instructed to stay inside the hangar.
- All other emergencies will be handled as necessary.
- Event coordinators will inform the emergency services.
- Event coordinators and police officers will direct emergency services when they arrive.

## **TRAFFIC MANAGEMENT PLAN**

See attached ramp layout for parking.

## **FIRST AID SERVICES**

First aid tent will be located near the dining area. It will be accessible for ambulances through the normal entrance.

## **MEDIA**

If there is media interest, the event managers, a member from the Commemorative Air Force, or legal counsel would speak to the media.  
Media Contact: Tristan Henderson | 612-296-6524 | [Tristan@fptuned.com](mailto:Tristan@fptuned.com)

## Contact Us:

Any questions at all please contact:

Further Performance  
207 Humboldt Ave. N.  
Minneapolis, MN 55405  
612-374-2604

Tristan Henderson  
[Tristan@fptuned.com](mailto:Tristan@fptuned.com)  
612-296-6524

Mike Olson  
[Mike@fptuned.com](mailto:Mike@fptuned.com)  
612-203-8288

Marnie Gamble  
[marnie.gamble@gmail.com](mailto:marnie.gamble@gmail.com)  
612-867-1913

Planning Commission Meeting Date:  <b>Wednesday, September 7, 2016</b>		<b>City of South St. Paul Planning Commission Report</b>	PC Agenda Item:  <b>3.D</b>
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<b>Project Name:</b>	<b>Danner, Inc. - Verderosa</b>		
<b>Site Address:</b>	600 Verderosa (also shown as 587 Verderosa)	<b>PID's:</b> 360260025020 360260062017 360260065010	
<b>Applicant:</b>	Danner Family Limited Partnership		
<b>Request:</b>	Consider the request for variances for minimum building size, conditional use permits for gas/diesel station and exterior storage, and an Interim Use Permit for temporary rock crushing operation		
<b>Proceeding:</b>	Public Hearing / Planning Commission Recommendation		
<b>Tentative City Council Meeting Date:</b>	September 19, 2016		
<b>Deadline:</b>	<b>October 11, 2016*</b> <small>*The City must act on this request by this 60-day review period deadline unless the city provides the applicant with written reasons for why additional time, up to a maximum of 120 days, is required. The City may extend the review period beyond the 120 days but only with the applicant's consent.</small>		
<b>Exhibits:</b>	A. Location Map, previous approvals B. <del>Correspondence from neighboring property owners</del> (None) C. Materials submitted by the applicant		

**BACKGROUND**

Surrounding Area			
Direction	Future Land Use Plan	Zoning	Existing Land Use
North	Light Industrial ("494 Corridor")	I: Industrial	Light Industrial: United Rentals
South	Interstate 494	N/A	Interstate 494
East	River / trail	N/A	River / trail
West	Light Industrial ("494 Corridor")	I: Industrial	Light Industrial: Pomp's
Subject Property Site Data			
<b>Future Land Use Plan</b>	Light Industrial ("494 Corridor")		
<b>Existing Land Use</b>	Vacant		
<b>Zoning</b>	I: Industrial		
<b>Property Size</b>	9.25 acres [effectively 6.81 acres] - (296,576 s.f)		
<b>Topography</b>	Moderate slope		

**ZONING SUMMARY:**

<b>Bulk Requirements</b>	<b>Required</b>	<b>Proposed</b>
<b>Setbacks:</b>		
Front yard	30 ft.	80 ft 50 ft to fuel canopy
Side yard (east/west)	20 ft.	67 ft.
Rear yard	20 ft.	N/A
Building Height: (access. bldg.)	35 ft max (w/o CUP)	25.66 ft
<b>Site Design</b>	<b>Required</b>	<b>Proposed</b>
<b>Parking Standards:</b>		
Parking stalls	61	61*
Accessible parking stalls	1**	2
<b>Parking Setbacks:</b>		
Street-side	20	20
Interior side	10	10
Exterior Building Materials:	Precast concrete – painted banding, brick veneer accent on street side, windows primarily on office area but shown throughout.	
<b>Other Critical Zoning Items</b>		
Floodplain	No (behind levee)	
Shoreland	Yes	
MNRRRA	Yes	
Utility easements	No	

*\*The plan notes that 37 of the service staging stalls are provided in the exterior storage area which would not meet the City Code for approved hard surface parking area.*

*\*\*ADA accessible parking spaces need to be striped and signed and include the required access aisle space. One space is required for parking lots with 25 spaces or less, 26 or more would require 2 accessible spaces.*

**EVALUATION OF THE REQUEST**

The applicant is proposing to relocate their operation from 843 Hardman Avenue to the undeveloped site at 600 Verderosa. The proposal would require extensive soil remediation which would occur in phases going from west to east. The western portion of the property would accommodate a 19,317 square foot building including approximately 3,500 square feet of office space. The building would include truck service and would have a lower level parts storage area under the office space. In addition, the western portion of the property would include fueling stations for gas and diesel. The building would face north toward Verderosa Avenue. The eastern portion of the property would be a screened exterior storage area and would also house a temporary concrete crushing operation and stockpiling. The concrete crushing and stockpiling would coincide with the removal of concrete material currently buried on a portion of the site which would then be crushed on site. The concrete crushing and stockpiling is anticipated to last for 4 years. Screening would be provided by either fences or trees depending on the location.

## A. VARIANCE/ CONDITIONAL USE PERMIT/ INTERIM USE PERMIT

Following are standards from the City's Zoning Code that apply to specifically to the application.

### 1. Zoning / Land Use

The property is guided for light industrial and is part of the "494 Corridor" which stresses an importance on aesthetics, reduced exterior storage, and commercial or light industrial uses representative of the positive development within the BridgePoint area over the last decade. The proposed repair use could be considered light industrial but the exterior storage, concrete crushing, and stockpiling run counter to the goals of the "494 Corridor". The site is tucked up against the Interstate 494 right of way which is significantly higher than the site and may help to hide the site to an extent except where traffic exits for Hardman Avenue which would look right into the back of the site. The applicant has proposed a mixture of fence and coniferous trees to help screen the site.

The property is zoned I: Industrial which allows office as a Permitted Use, truck and vehicle repair as a Conditional Use, and exterior storage as a Conditional Use, and temporary on-site rock crushing (when used for surcharging the land as an integral part of a land reclamation approved by the City Engineer) as a Conditional Use.

Another zoning issue is the size of the building which at just over 19,000 square feet is only 6.5% of the usable parcel size (6.86 acres) where the Code would require a minimum building size of approximately 60,000 square feet. Since the concrete crushing and stockpiling on the eastern portion of the site are interim uses it is possible that portion of the site could eventually be developed using similar size site to that of the proposed repair building then there should be space to accommodate an additional 1-2 principal buildings. If that were to occur, it would bring the building coverage on the site to be consistent with Code requirements otherwise a significant variance for minimum building size would be required and findings to support the variance would be needed. Additionally, platting the property to accommodate development on the site as 2-3 parcels would also be advisable.

### 2. Exterior materials

The exterior on the plans shows precast concrete with painted banding as the primary building material. The building design includes a brick veneer on the front face of the building facing Verderosa Avenue and also has many windows on that side of the building. Windows are also used on the east and west sides of the building in the truck service area. A metal canopy is used as an accent material around the entries to the office areas.

3. Parking/Parking Surfaces / Stormwater Treatment

The site plan shows 24 designated parking spaces while the notes indicate that another 37 spaces would be in the exterior storage area. Unfortunately, those are not shown on the plan but there certainly is space to accommodate them. There is also additional space around the building, along the fence line and elsewhere on the developed portion of the site to provide more parking while allowing space for future redevelopment of the land further east on the site. The civil plans will need to be revised to show the location of the parking or "proof of parking" along with surfaces and landscape islands.

The City Code states that parking surfaces be paved and include curbing. The narrative indicates that the exterior storage area would be bituminous (which would be consistent with exterior storage requirements for surfaced area) however the plans do not show that the parking lot would be bituminous (just assumed), where curbing is located, landscaped planting islands and how stormwater treatment would be handled. If over 1 acre of impervious is disturbed with the development (which appears to be the case) then stormwater ponding or alternative means would be required for dealing with the stormwater.

Revised civil plans would be required showing the parking lot area and stormwater treatments. These plans will be subject to the review and approval of the City Engineer.

4. Odors

The proposed use as a truck repair facility with exterior storage is not expected to create odors which would impact the area. It is not known whether excavation would release odors due to the history of the site as a location of ponch manure from the former meat packing plants, but those odors would occur due to the excavation no matter what the end use would be.

5. Building Design and Architectural Standards / Height Requirements

The proposed building is predominantly precast concrete with some painted banding and some brick veneer and glass on the front face of the building. Additional windows are shown on the east and west sides of the building providing some natural light into the space and helping to break up the look of the building. The overhead doors for the service area are on the east and west sides so they do not face the street. The site also has a metal fuel service canopy in front of the building for the gas fueling area and diesel fueling canopy to the east side of the building.

Industrial district requirements call for no structure to exceed 35 feet without a Conditional Use Permit. The proposed building would be no taller than 26 feet. The site is also in the MNRRRA/MRCCA overlay zone area and the height requirement of 35 feet would also need to be adhered to for the stockpiling proposed on the eastern portion of the site.

6. Correspondence from Neighboring Property Owners

Staff had not received any correspondence from neighboring property owners prior to the writing of this report.

7. Landscaping

Landscaping standards for the Industrial district require at least 15% of the site to be landscaped. The landscaping plan and impervious area, mentioned earlier are incomplete. Revised landscaping and civil plans will be required. A list of landscape plantings has been included that shows a mixture of deciduous and coniferous trees show planted around the site. Generally, the south side of the site, facing 494, would include coniferous trees which would serve the dual purpose of landscaping and also screening the exterior storage area.

8. Variances Proposed in the Application

Minimum building size of 20% of the site is required where the application contemplates only 6.5%, a variance of 13.5% from the standard for the Industrial District.

9. Variance Findings

In variance cases the City is required to make findings in regard to practical difficulties as used in connection with the granting of a variance as defined by State Statute 462.357, subd. 6 and in City Code Section 118-39. The City must make the following findings in considering approval of a variance:

- a. *The variance is in harmony with the general purpose and intent of the ordinance*
- b. *The terms of the variance are consistent with the Comprehensive Plan, and*
- c. *The applicant for the variance establishes that there are practical difficulties in complying with the ordinance. (Economic considerations alone do not constitute practical difficulties). Practical difficulties as used in connection with the granting of a variance means that:
  - i. *The property owner proposes to utilize the property in a reasonable manner.*
  - ii. *The plight of the property owner is due to circumstances unique to the property that were not created by the property owner, and**

iii. *The variance will not alter the essential character of the neighborhood.*

Staff has reviewed the proposal and while the site has some unique attributes and practical difficulty that may dictate granting a variance the amount of the variance necessary would be far too large. Revising the plans to contemplate the future development of the eastern portion of the site and perhaps amending the application to be a Planned Unit Development (PUD) would be advisable. That would allow the City to consider the development as a whole rather than piece by piece for each Variance, CUP, IUP, etc.

10. Conditional Use Permit Findings:

The applicant is seeking a Conditional Use Permit (CUP) for a gas/diesel fueling station on the site and for exterior storage (not the stockpiling/rock crushing that will be considered as an Interim Use Permit on the subject property at 600 Verderosa Avenue. The Council may grant a Conditional Use Permit, if the following criteria can be met:

- ✓ *The proposed conditional use conforms to the general purpose and intent of the zoning code.*
- ✓ *The conditional use will not substantially diminish or impair property values within the neighborhood, and in consideration of this question the comparison of the use shall be with respect to uses that are permitted without a Conditional Use Permit in the district which the use is located.*
- ✓ *The conditional use will not impede the normal and orderly development and improvement of property in the neighborhood for uses permitted in the district affected.*
- ✓ *Adequate utilities, access roads, streets, drainage, and other necessary facilities have been provided.*
- ✓ *Adequate measures have been or will be taken to provide ingress and egress in such a manner as to minimize traffic congestion and hazards in the public streets.*

Staff has reviewed the proposal and determined that the use conforms to the general purpose of the Zoning Code and with conditions should not substantially diminish or impair property values, will not impede the normal and orderly development of property in the neighborhood, already has adequate utilities, ingress and egress are not impacted.

11. Interim Use Permit Findings

The applicant is seeking an Interim Use Permit (IUP) to have stockpiling and rock crushing on the site for a four-year period that would expire on December 31, 2020. City Code Section 118-41, states that the City Council may grant an interim use permit for the interim use of property, if the following criteria can be met:

Interim Use Permit Criteria:

- ✓ *The use conforms to the zoning regulations.*
- ✓ *The date or event that will terminate the use can be identified with certainty.*
- ✓ *Permits for the use will not impose additional costs on the City, if it is deemed necessary for the City to take the property in the future; and.*
- ✓ *The user agrees in writing to any conditions that the City Council deems appropriate for permission of the interim use.*

City staff has determined that the proposed interim use would meet the criteria. First, the proposed use would allow for development of a site that will need significant soil remediation. The Interim Use would allow the crushing of concrete from a portion of the site and stockpiling dirt for use in the cleanup of the site which would allow for development of the remainder of the site. Second, the use will terminate after December 31, 2020. Third, the permit would not impose additional costs should it be necessary to acquire the property, the site is not a site that the City is planning to acquire in the future. Finally, the conditions are listed in staff's recommendation and would be incorporated into the resolution to approve the Interim Use Permit.

## **ALTERNATIVES**

### **The Planning Commission has the following actions available on the proposed application:**

- A. **Approval.** If the Planning Commission has review the application and determined that the application is consistent with the Variance findings (see p. 4 of this report) and the Conditional Use Permit findings (see p.5 of this report), then staff would recommend the following conditions for a *recommendation* for approval:
- **(Step 1) Findings:** The Planning Commission would need to include findings (see the section above) that the proposed Conditional Use Permits, Interim Use Permit, and Variance would not have an adverse impact on other properties and the general area or that potential impacts would be mitigated through specific conditions. Additionally, the Commission would need to include findings for the Variances to consider whether there is practical difficulty that would apply.
  - **(Step 2) Recommendation for Approval:** Approval of the **Variances** of 13.5 % for the minimum building size, a **Conditional Use Permit** for a gas/diesel fueling station, a **Conditional Use Permit** for exterior storage of equipment, and an **Interim Use Permit** for temporary concrete/rock crushing and stockpiling feet for the property located at 600 Verderosa Avenue, subject to the following conditions:
    - 1) **Compliance with Plan Submittals.** The site shall be utilized in substantial conformance, in the reasonable opinion of the City Council, with the application, narratives, and with the following plans on file with the Engineering Department:
      - a) **Application / Narrative (Danner, Inc.)** **dated 08/02/2016**
      - b) **Site, Building and Landscape Plans (Lampert Architects)** **dated 07/27/2016**

- 2) **Building Permits Required.** Building permits are required for the proposed improvements. All building plans and specifications are subject to the review and approval of the City Building Official and South Metro Fire Marshal.
- 3) **Parking and ADA Accessible Parking.** The applicant shall stripe the parking spaces and shall sign and stripe the parking lot to provide ADA accessible parking spaces at the building site.
- 4) **Revised Civil and Landscaping Plans.** The applicant shall submit revised landscaping plans showing the location of paved surfaces, curbing, and landscaping. Plans will also need to indicate how stormwater would be treated to comply with Code.
- 5) **Approvals from other agencies.** The applicant shall obtain all necessary approvals from other applicable agencies such as Mender, MnDOT, and Dakota County.
- 6) **Revised Site Plans.** The applicant shall submit revised site plans showing specific locations for exterior storage of equipment, concrete/rock crushing and stockpiling.
- 7) **Screening/Fencing.** Dumpsters shall be screened in accordance with City Code, such screening is subject to the review and approval of the City Planner. Exterior storage shall be screened behind wood, vinyl, or composite privacy fence or coniferous trees and shrubs. Barbed wire security fence is prohibited.
- 8) **Stormwater Maintenance Agreement.** The applicant will need to maintain their stormwater system to ensure that it functions properly. A stormwater maintenance agreement will be required which will explain steps to proper stormwater system maintenance.
- 9) **Compliance with Laws and Approvals.** The property must remain compliant with all federal, state, and local laws and ordinances and all prior City approvals.
- 10) **Term of the Interim Use Permit.** The Interim Use Permit for concrete/rock crushing and stockpiling shall run for a term of 4 years, expiring on December 31, 2020. This interim use shall be limited to the eastern portion of the property as shown on the revised site plan.
- 11) **Termination of the Variance, Conditional Use Permit, and Interim Use Permit.** The Variance, Conditional Use Permit, and Interim Use Permit will terminate if improvements have not substantially begun within 1-year from the date of approval of the Variance, Conditional Use Permit and Interim Use Permit. The violation of any condition of approval shall terminate the Variance, Conditional Use Permit, and Interim Use Permit.

B. **Denial.** If the Planning Commission does not favor the proposed application or portions thereof, the above requested should be recommended for denial. If the Planning Commission recommends denial, then findings of the basis for denial should be given.

- Recommendation for Denial: Denial of the proposed **Conditional Use Permit, Interim Use Permit, and Variance** for the property located at 600 Verderosa Avenue for the following reasons:

1) \_\_\_\_\_

#### **STAFF RECOMMENDATION**

The application involves a number of different requests and would require 2 Conditional Uses, a variance for the building size and an Interim Use Permit for the temporary rock crushing and stockpiling. There are a number of plans that need to be revised to account for requirements and the current proposal does little to discuss potential future development of the eastern portion of the site. Since part of the application is for an Interim Use it would seem only prudent to also have a discussion about the longer term plan for the proposed site. Staff believes that the best way to address these issues would be for the applicant to withdraw the application in its current format and resubmit the application as a Planned Unit Development (PUD) and plat. The PUD would allow the City to review the site as one development rather than several piecemeal items and would provide better flexibility in dealing with things like the longer development timeline for the eastern portion of the site.

The proposed use of the site for exterior storage, stockpiling, and concrete/rock crushing is contrary to the vision for properties in the "494 Corridor" from the City's current Comprehensive Plan which looked to lessen exterior storage and improve aesthetics adjacent to 494. However, the site has substantial soil issues that and the proposed use would provide an opportunity to create future development opportunities on the site. The property is also large enough that it could be split into another two development parcels. Based on the proposed use of the site as a repair facility and the adjacent repair facility it is likely that any additional development on the site would be drawn to that site for similar use.

Staff recommends tabling the request to the October Planning Commission meeting to allow the applicant time to provide the requested information. Staff would also recommend withdrawing the current application and resubmittal as a Planned Unit Development. If considered as submitted and without the additional items noted, then staff would have to recommend denial.

Respectfully Submitted,

*Peter Hellegers*

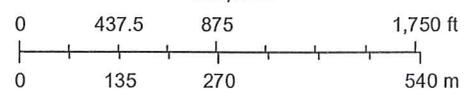
Peter Hellegers, City Planner

# Danner - Verderosa Site and Hardman



August 8, 2016

1:9,600



Schools  
Property Information  
Political Administrative

A<sub>2</sub>

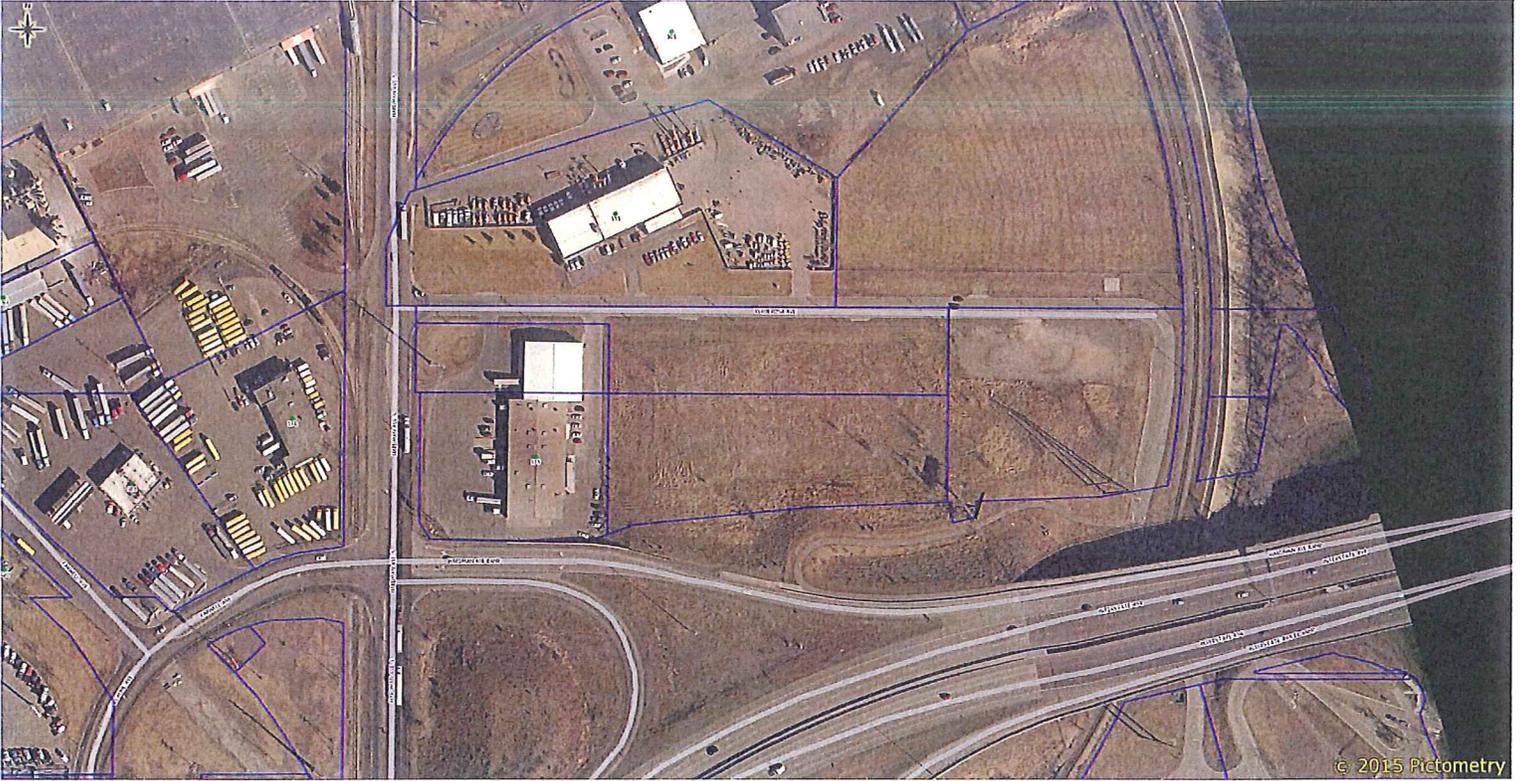
# 600 Verderosa



03/15/2015

# 600 Verderosa

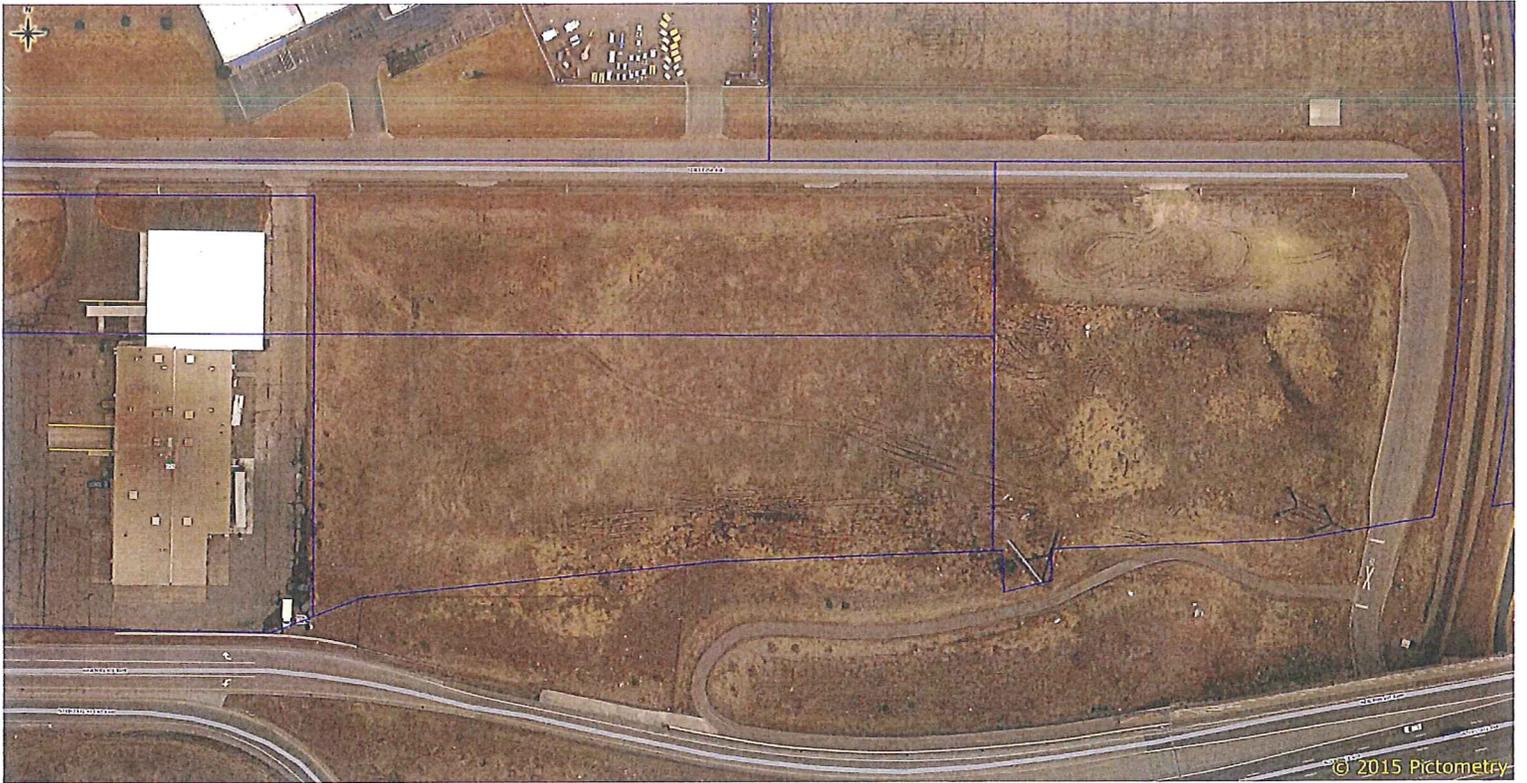
A<sub>3</sub>



03/17/2015

A4

# 600 Verderosa



03/15/2015



# South St. Paul

A5

**Meeting Date:** September 7, 2016

**To:** South St. Paul Planning Commission

**From:** Ryan Garcia, Economic Development Manager

**Subject:** Economic Development Advisory Board (EDAB) Comments – Verderosa Development Concept

## Overview

At its regularly scheduled meeting on August 16, 2016, the Economic Development Advisory Board (EDAB) reviewed a development concept and initial staff findings as related to the redevelopment concept proposed by Danner, Inc. for the roughly 7 acre property on Verderosa Avenue. Staff is presenting the Planning Commission with EDAB's feedback as an informational supplement for consideration at the public hearing for this item.

## Summary of EDAB Comments and Concerns

EDAB recognized the opportunity to improve the long-term viability of development of the site in question and improve the City's tax base, but hoped that the formal submission would offer additional clarity on several issues, as summarized below:

- Given the nature of the proposed operation, the volume and intensity of heavy vehicular traffic should be expected to increase, likely in a manner consistent with Danner, Inc.'s current operations south of 494. EDAB members noted that existing operations routinely introduce debris, dust, and dirt on the Hardman Ave. South roadway and access routes.
- The most significant aesthetic impacts of concern cited by EDAB members were related to the visual impacts (stockpiling of material), especially as they are viewed from I-494 and the adjacent trail corridor as well as air and water quality impacts from rock crushing-related dusts, particulates, and mitigation measures.
- The Concept as it was presented in August indicated that crushing operations were anticipated for "approximately 4 years". EDAB was interested in gaining an understanding of how firm that time commitment was, whether that timeline would be integrated into any approvals, and what the long-term plan for the site entails. EDAB noted the existing operation has had its "interim" use permit extended in the past and felt that such an approach would not be appropriate or desirable for the Verderosa location.
- EDAB was hopeful that if the concept receives City approval, such approval would include clear and firm conditions that sought to minimize the impacts cited above while emphasizing the expectation that the long-term objective is to address soil and environmental conditions at the site and enhance the developability of the property.

C<sub>1</sub>

To City Council,

AUG 02 2016

We are going to ask for a C.U.P for extra storage of equipment, trucks and for our fueling station. We are also asking for an I.U.P for material to stockpile on site, before we start digging out the demolition material. There is about 400,000 yards of material that need to be dug out and hauled away. We want to crush the concrete on site for approximately 4 years. It will take about that long to get that done. The removal and haul out of material that is stockpiled on the property, will be placed there in the summertime, except when the building is being constructed. That will have to be done in the winter, because it should be put up in March.

The building will be a prefabricated concrete building. We will attach pictures along with our proposed plans.

The outside area that will be removed will be screened of brush and trees. This lot should be about 8-10 feet lower than the path. There will be a wall constructed on the south side of the property. There will be a chain link fence put on top of the storage area. When completed it will be an all impervious surface.

Being that the property is all contaminated. The storm water will have to go directly into a storm pipe that will have to be moved onto the property. After the parking lot is done we will have to do something different.

The signage will remain the same as it is on our property now.

I can explain more if I am aloud to come to a work session and answer any questions you may have.

A handwritten signature in black ink, appearing to read "Malone". The signature is fluid and cursive, with a large loop at the end.

C2

# VERDEROSA AVENUE

LAMPERT ARCHITECTS

420 Summit Avenue  
St. Paul, MN 55102  
Phone: 763.755.1211 Fax: 763.757.2849  
lampert@lampert-arch.com

ARCHITECT CERTIFICATION:  
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

**PRELIMINARY NOT FOR CONSTRUCTION**

AUG 02 2016

**DANNER TRUCK SERVICE**  
South St. Paul, Minnesota

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Leonard Lampert Architects, P.A.  
Project Designer: JAMES B  
Drawn By: JRB  
Checked By: LL

Revisions

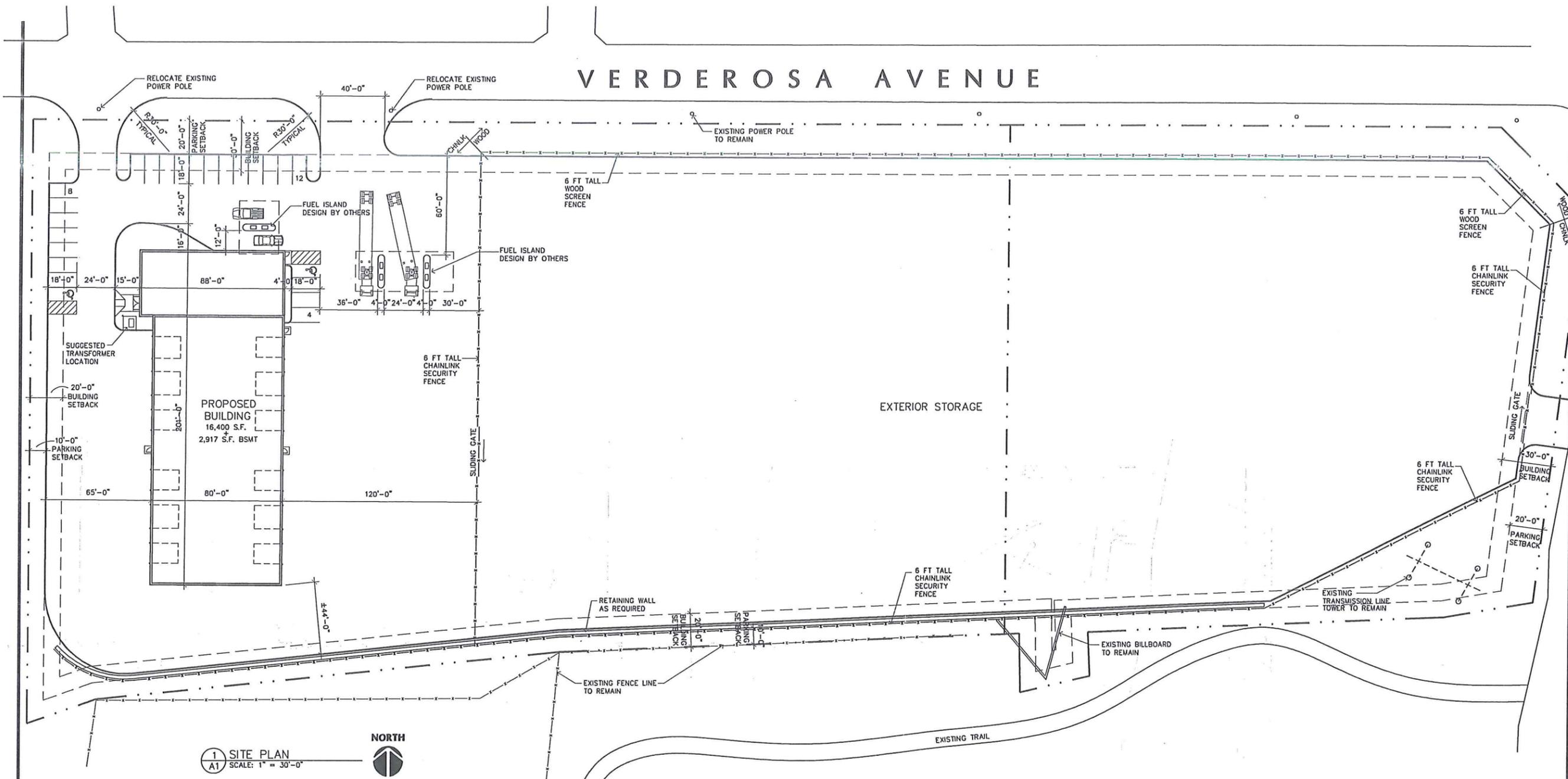
7/8/16	PRELIMINARY
7/27/16	CITY SUBMITTAL

SITE PLAN

Sheet Number

# A1

Project No. 160623-1



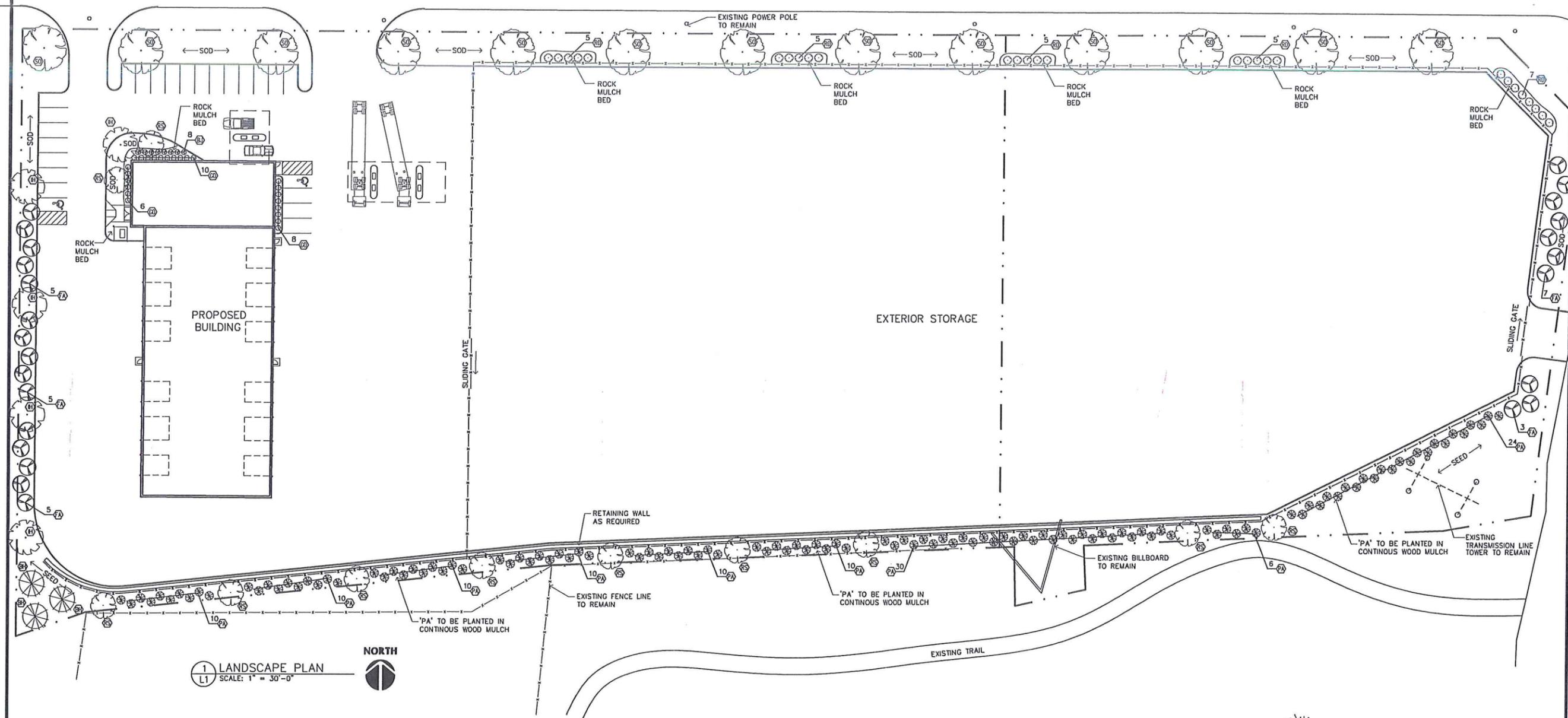
1 SITE PLAN  
A1 SCALE: 1" = 30'-0"

**SITE DATA**  
 ZONING - I, INDUSTRIAL DISTRICT  
 CUP REQUIRED FOR TRUCK REPAIR AND EXTERIOR STORAGE  
 LOT SIZE - ±296,576 S.F. = 6.81 ACRES  
 BUILDING  
 16,400 S.F. TRUCK REPAIR FACILITY + 2,917 S.F. BASEMENT  
 BUILDING COVERAGE  
 16,400 S.F./ 296,576 S.F. = 5.5% < 40%  
**PARKING DATA**  
 OFFICE AREA  
 3,520 S.F. AT 1/300 = 12 STALLS  
 REPAIR GARAGE  
 12 BAYS AT 3/BAY = 36 STALLS  
 1 PER EMPLOYEE = 12 STALLS  
 STORAGE AREA  
 2,917 S.F. AT 1/2000 = 1 STALLS

61 STALLS REQUIRED  
 24 STALLS PROVIDED  
 37 SERVICE STAGING STALLS PROVIDED IN EXTERIOR STORAGE  
 61 TOTAL STALLS PROVIDED

NOTE: BOUNDARY MUST BE VERIFIED WITH CURRENT SURVEY

# VERDEROSA AVENUE



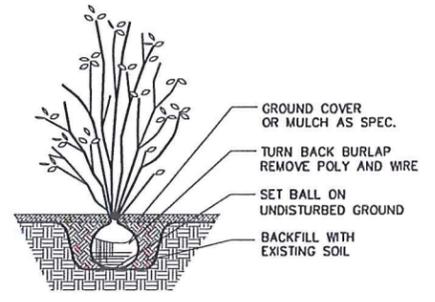
PRELIMINARY  
NOT FOR  
CONSTRUCTION

**DANNER TRUCK SERVICE**  
South St. Paul, Minnesota

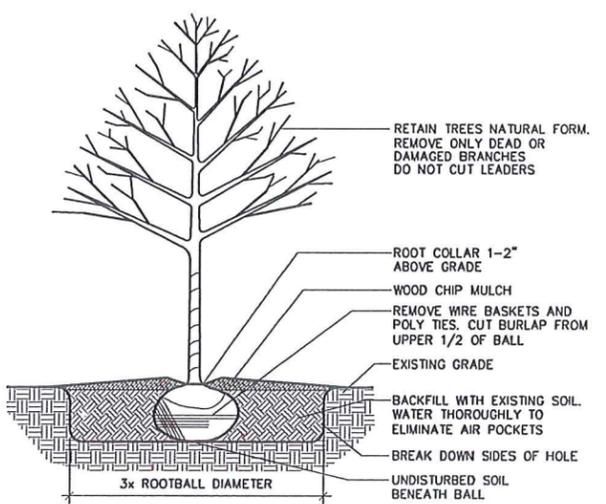
1 LANDSCAPE PLAN  
SCALE: 1" = 30'-0"  
NORTH

QTY.	KEY	COMMON NAME	BOTANICAL NAME	SIZE	MTHD	REMARKS
13	SO	SWAMP WHITE OAK	QUERCUS BICOLOR	2.5"	BB	
5	IH	IMPERIAL HONEYLOCUST	BETULA NIGRA	2.5"	BB	
11	RS	RED SPLENDOR CRAB	MALUSxHYBRIDS 'RED SPLENDOR'	1.5"	BB	
3	BH	BLACK HILLS SPRUCE	PICEA GLAUCA DENSATA	6 FT	BB	
25	FA	FAT ALBERT BLUE SPRUCE	PICEA PUNGENS	6 FT	BB	
27	RD	RED TWIGGED DOGWOOD	CORNUS BAILEYI 'CARDINAL'	#5	CONT.	
114	PA	PYRAMIDAL ARBORVITAE	THUJA OCCIDENTALIS	#5	CONT.	
24	GD	GARDEN GLOW DOGWOOD	CORNUS HESSI GARDEN GLOW	#5	CONT.	
8	BJ	BLUE CHIP JUNIPER	JUNIPERUS HOR. BLUECHIP	#5	CONT.	

- LANDSCAPE NOTES:**
- SEED/SOD AREAS AS SHOWN ON THE PLAN.
  - PLANTINGS TO BE PLACED IN ROCK MULCH BED. PLANTING BED TO BE SEPARATED FROM SODDED AREAS BY BLACK VINYL EDGER.
  - UNDERGROUND IRRIGATION SYSTEM TO BE PROVIDED FOR ALL FOUNDATION PLANTINGS AND SODDED AREAS. PROVIDE MOISTURE SENSOR.
  - SITE IS VOID OF SIGNIFICANT EXISTING VEGETATION



3 SHRUB PLANTING DETAIL  
SCALE: NOT TO SCALE



2 TREE PLANTING DETAIL  
SCALE: NOT TO SCALE

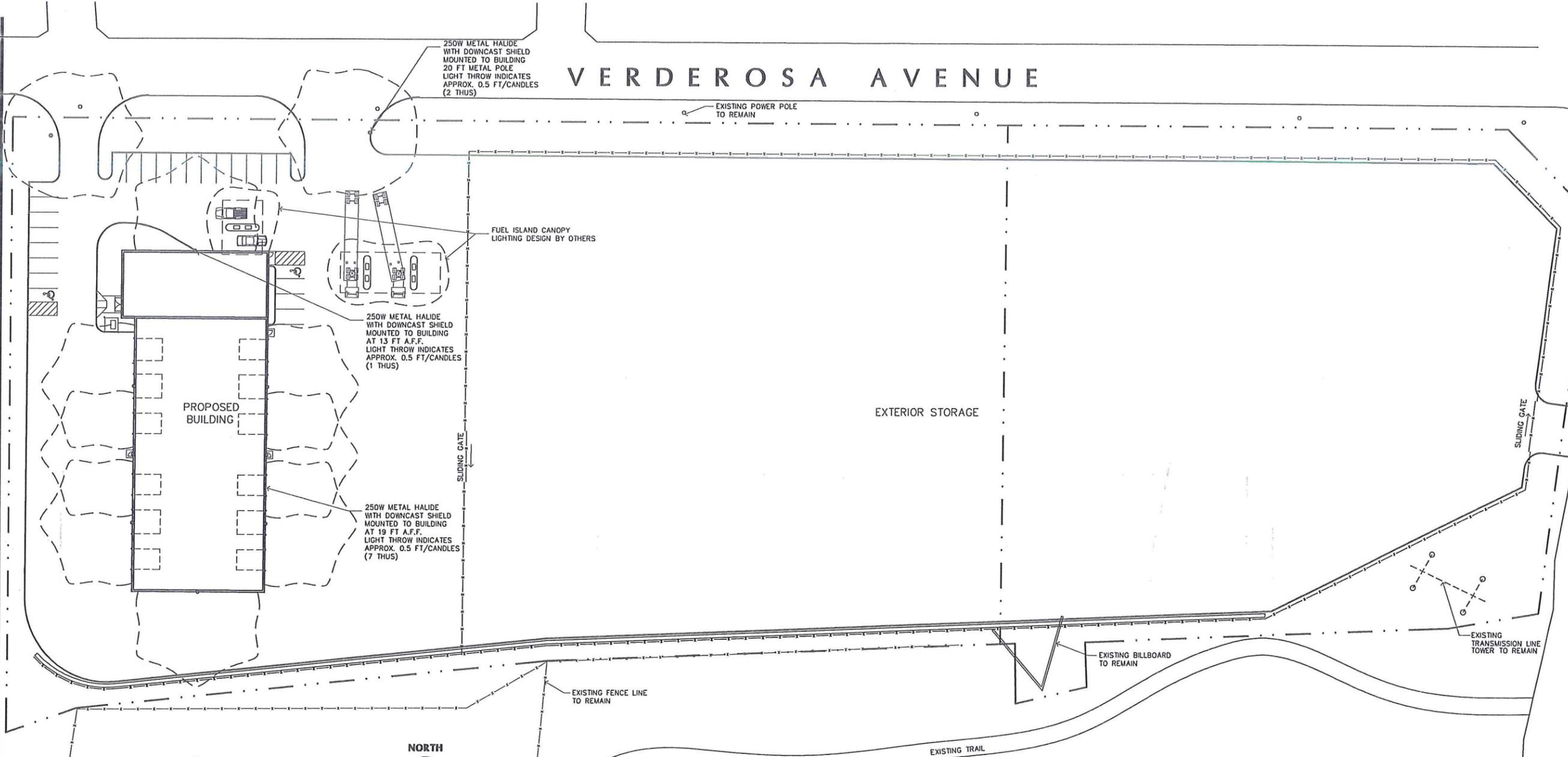
Drawn By: JRB  
Checked By: LL  
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7/8/16 PRELIMINARY  
7/27/16 CITY SUBMITTAL

LANDSCAPE PLAN  
Sheet Number

**L1**

C4

# VERDEROSA AVENUE



PRELIMINARY  
NOT FOR  
CONSTRUCTION

**DANNER TRUCK SERVICE**  
South St. Paul, Minnesota

1 SCHEMATIC SITE LIGHTING PLAN  
L2 SCALE: 1" = 30'-0"



- PLAN NOTES:**
1. LIGHTING SHOWN FOR SCHEMATIC PURPOSES ONLY. LIGHTING IS DESIGN-BUILD BY ELECTRICAL CONTRACTOR. VERIFY SIZE, STYLE, QUANTITY, AND LOCATION W/ OWNER AND CONTRACTOR. PROVIDE LIGHTING PHOTOMETRICS IF NECESSARY AT CITY'S REQUEST.
  2. ALL LIGHTING IS TO BE SHIELDED/DIRECTED AWAY FROM ADJACENT PROPERTIES AND STREET RIGHT OF WAYS.
  3. PROVIDE EMERGENCY EXIT LIGHTING AT ALL EXIT DOORS.

Drawn By: JRB  
Checked By: LL

Revisions

7/8/16	PRELIMINARY
7/27/16	CITY SUBMITTAL

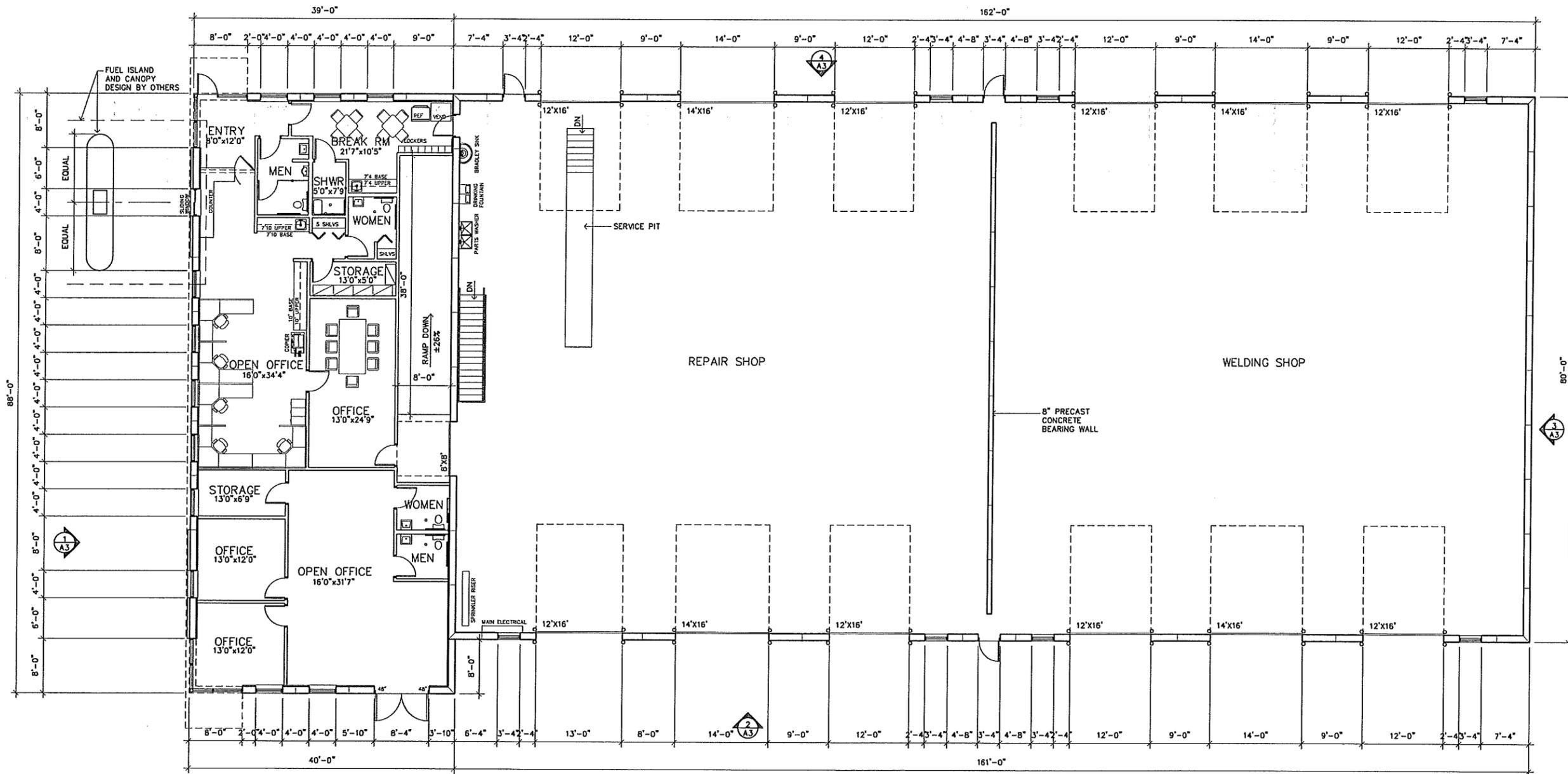
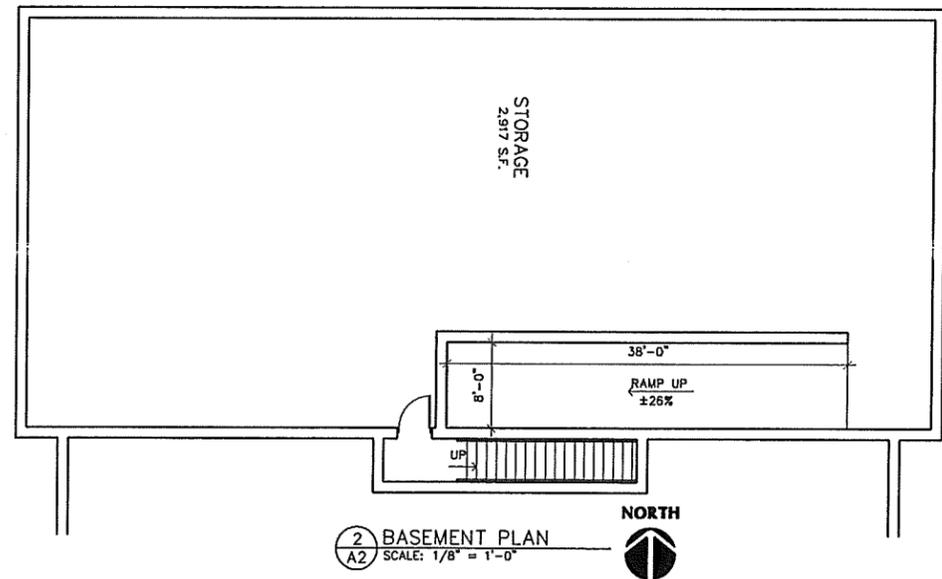
## SCHEMATIC SITE LIGHTING PLAN

Sheet Number

# L2

Project No. 160623-1

Filename: DANNER TRUCK SERVICE\DANNER TRUCK SERVICE-A2



LAMPERT ARCHITECTS

420 Summit Avenue  
St. Paul, MN 55102  
Phone: 763.755.1211 Fax: 763.757.2849  
lampert@lampert-arch.com

ARCHITECT CERTIFICATION:  
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

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South St. Paul, Minnesota

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Project Designer: JAMES B  
Drawn By: JRB  
Checked By: LL

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FLOOR PLAN

Sheet Number

A2

Project No. 160623-1

C6

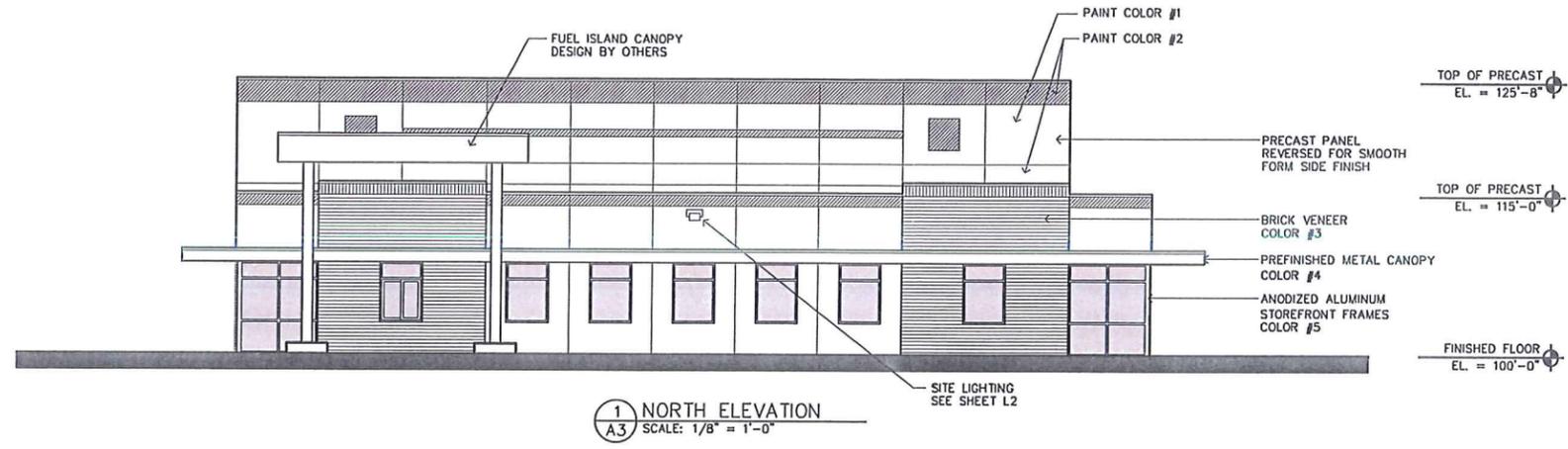


LAMPERT ARCHITECTS

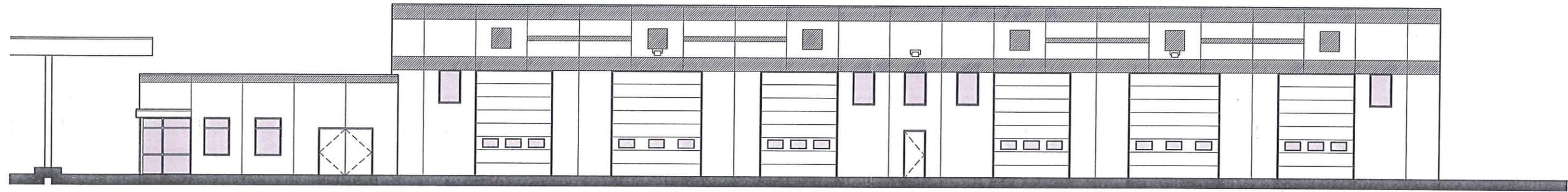
420 Summit Avenue  
St. Paul, MN 55102  
Phone: 763.755.1211 Fax: 763.757.2849  
lampart@lampart-arch.com

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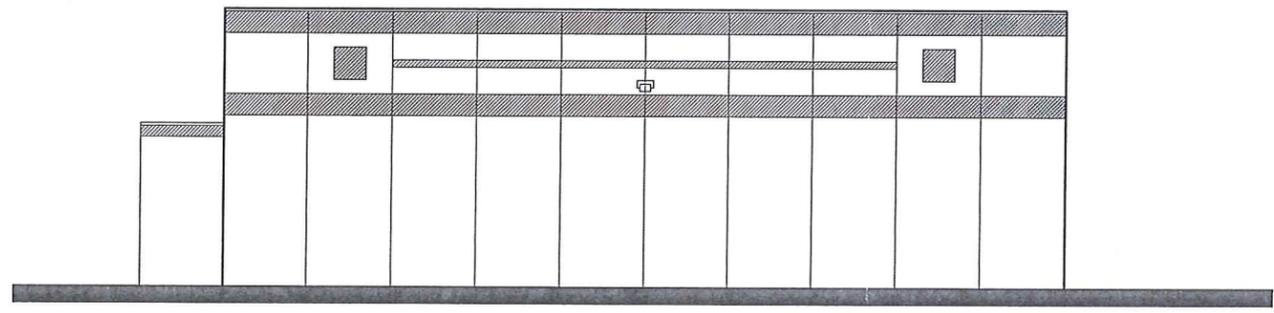
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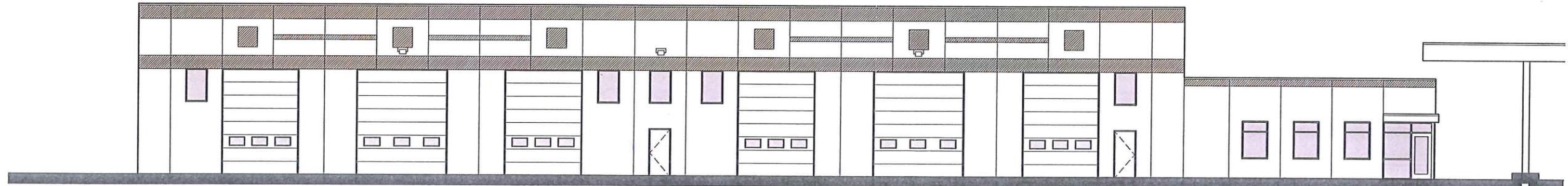
1 NORTH ELEVATION  
A3 SCALE: 1/8" = 1'-0"



2 WEST ELEVATION  
A3 SCALE: 1/8" = 1'-0"



3 SOUTH ELEVATION  
A3 SCALE: 1/8" = 1'-0"



4 EAST ELEVATION  
A3 SCALE: 1/8" = 1'-0"

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South St. Paul, Minnesota

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Project Designer: JAMES B
Drawn By: JRB
Checked By: LL
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7/21/16 PRELIMINARY
7/27/16 CITY SUBMITTAL

BUILDING ELEVATIONS

Sheet Number

A3

Project No. 160623-1

Filename: DANNER TRUCK SERVICE\DANNER TRUCK SERVICE-A3

Planning Commission Meeting Date:  <b>Wednesday, September 7, 2016</b>	 <b>City of South St. Paul</b> <b>Planning Commission Report</b>	PC Agenda Item:  <b>3.E</b>
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<b>Project Name:</b>	<b>Twin City Tanning Building Addition</b>	
<b>Site Address:</b>	501 Malden St.	<b>PID:</b> 360340001021
<b>Applicant:</b>	Gunderson Construction (on behalf of Twin City Tanning)	
<b>Request:</b>	Consider the request for a variance for a proposed 6,254 s.f. building addition	
<b>Proceeding:</b>	Public Hearing / Planning Commission Recommendation	
<b>Tentative City Council Meeting Date:</b>	September 19, 2016	
<b>Deadline:</b>	<b>October 7, 2016*</b> <small>*The City must act on this request by this 60-day review period deadline unless the city provides the applicant with written reasons for why additional time, up to a maximum of 120 days, is required. The City may extend the review period beyond the 120 days but only with the applicant's consent.</small>	
<b>Exhibits:</b>	A. Location Map, previous approvals B. <del>Correspondence from neighboring property owners</del> (None) C. Materials submitted by the applicant	

**BACKGROUND**

Twin City Tanning is proposing a 6,254 square foot addition to the southwest side of their existing building. The proposed addition would allow them to maintain an existing collagen operation that they have in the existing building but the building does not have the required clearance heights. The proposed addition would be 17.6 feet from the property line and thus would require a variance of 12.4 feet from the Industrial District setback requirements. In addition to the proposed addition the applicants are also proposing to build a 23 space parking lot at the southwest corner of the property that would replace parking currently in the area of the proposed addition. The proposed parking lot would require a setback variance for the southeasterly spaces.

Subject Property Site Data	
<b>Future Land Use Plan</b>	Light Industrial ("494 Corridor")
<b>Existing Land Use</b>	Industrial
<b>Zoning</b>	I: Industrial
<b>Property Size</b>	6.16 acres (268,330 total square feet)
<b>Topography</b>	Generally flat, slight amount of slope from Malden Street down to property

**ZONING SUMMARY:**

<b>Bulk Requirements</b>	<b>Required</b>	<b>Existing</b>	<b>Proposed</b>
Setbacks:			
Front yard	30 ft.	27.3 ft	17.6 ft
Side yard (east/west)	20 ft.	0 ft	N/A
Rear yard	20 ft.	0 ft	N/A
Building Height: (access. bldg.)	35 ft max (w/o CUP)	23 ft. appx.	26 ft
<b>Site Design</b>	<b>Required</b>	<b>Existing</b>	<b>Proposed</b>
Parking Standards:			
Parking stalls	Appx. 43-59	23*	23
Accessible parking stalls	2-3	1?	?**
Parking Setbacks – to street-side property lines:	20	20	0
Exterior Building Materials:			
<b>Other Critical Zoning Items</b>			
Floodplain		Yes	
Shoreland		No	
MNRRRA		Yes	
Utility easements		No	

*\*The existing parking lot has a space for car parking that generally accommodates 23 vehicles. Approximately the same number of cars park in the end of the Malden Street right of way which acts as another makeshift parking lot. It appears that some cars park in the large paved truck area near the existing parking lot.*

*\*\*ADA accessible parking spaces need to be striped and signed and include the required access aisle space. One space is required for parking lots with 25 spaces or less, 26 or more would require 2 accessible spaces, 51 would require 3.*

**EVALUATION OF THE REQUEST**

**A. VARIANCE**

Following are standards from the City's Zoning Code that apply to specifically to the application.

1. Zoning / Land Use

The property is guided for light industrial and is part of the "494 Corridor" which stresses an importance on aesthetics, reduced exterior storage, and commercial or light industrial uses representative of the positive development within the BridgePoint area over the last decade.

The property is zoned I: Industrial which allows manufacturing inside of a building as a permitted use. The building has some non-conforming setbacks but Code will allow additions to such structures provided the additions are consistent with Code (or they obtain variances to be deemed consistent). The property is not protected by the levee/floodwall and while over 2,200 feet away

from the river it may be subject to some flooding. The development will need to be consistent with the City's floodplain ordinance. There also is a fair amount of exterior storage just west of the building and east of the drive lane that would need to be removed.

2. Exterior materials

The exterior on the plans shows a precast concrete wall and some windows on the proposed addition. While the addition is under the size to be required to follow the architectural standards, the location in the "494 Corridor" and surrounding development highlight the need to review the aesthetics. Windows on the south side (street frontage) and architectural elements such as banding or texture variation are recommended.

3. Parking/Parking Surfaces

The City Code states that parking surfaces be paved and include curbing. The proposed parking lot is bituminous and is shown with curbing and gutter. Drainage from the parking lot would outlet to the landscaped area to the northwest. Parking for the use is currently in the area of the proposed addition and the dead end of Malden/Maltby functions as a second parking lot area which cannot be used toward parking requirements since it is in the right-of-way. Functionally there should be little impact to the parking situation as the displaced parking spaces would be replaced and the existing parking would continue out in the street. However, since Code considers the parking spaces on the property the City should consider "proof of parking" in the area north of the proposed parking lot where additional parking could be accommodated.

In addition the plans show a 6-foot sidewalk in the Malden Street right-of-way to connect the proposed parking with the front of the TCT building addition. Since this is a private sidewalk it should be kept within the property lines to prevent confusion that it is a public sidewalk. The large existing concrete driveway apron near the proposed parking lot will also need to be removed.

4. Odors

The proposed addition would be refrigerated space to accommodate an additional process to the collagen processing that TCT currently handles at the site. The process does not involve any burning/cooking and would only have odors when the drums where the materials are stored would be opened. The applicants had St. Croix Sensory and American Engineering Testing to monitor the odor from the process. Air handling/filtering systems in the new refrigerated areas will have filters that will limit potential release of odors and those filters will need occasional replacement.

5. Correspondence from Neighboring Property Owners

Staff had not received any correspondence from neighboring property owners prior to the writing of this report.

6. Variations Proposed in the Application

The City's Zoning Code (Sec. 118-129) requires a front yard setback of 30 feet where the proposed addition has only 17.6 feet leaving a variance of 12.4 feet. Parking setbacks of 20 feet are required from property lines for any frontage on a street. Also of note; parking required for an industrial building of this size would be 43-59 parking spaces where only 23 spaces are provided. The applicant will need to either construct additional off-street parking for up to an additional 36 parking spaces within the property lines or show proof of parking for the same number of spaces. ADA parking spaces will be required and will need to be signed and striped accordingly.

7. Variance Findings

In variance cases the City is required to make findings in regard to practical difficulties as used in connection with the granting of a variance as defined by State Statute 462.357, subd. 6 and in City Code Section 118-39. The City must make the following findings in considering approval of a variance:

- a. *The variance is in harmony with the general purpose and intent of the ordinance*
- b. *The terms of the variance are consistent with the Comprehensive Plan, and*
- c. *The applicant for the variance establishes that there are practical difficulties in complying with the ordinance. (Economic considerations alone do not constitute practical difficulties). Practical difficulties as used in connection with the granting of a variance means that:*
  - i. *The property owner proposes to utilize the property in a reasonable manner.*
  - ii. *The plight of the property owner is due to circumstances unique to the property that were not created by the property owner, and*
  - iii. *The variance will not alter the essential character of the neighborhood.*

Staff has reviewed the proposal and determined that the use conforms to the general purpose of the Zoning Code and with conditions should not substantially diminish or impair property values, will not impede the normal and orderly development of property in the neighborhood, already has adequate utilities, and as an existing building has adequate ingress and egress.

**ALTERNATIVES**

**The Planning Commission has the following actions available on the proposed application:**

A. **Approval.** If the Planning Commission has review the application and determined that the application is consistent with the Variance findings (see p. 4 of this report) and the Conditional Use Permit findings (see p.5 of this report), then staff would recommend the following conditions for a *recommendation* for approval:

- (Step 1) Findings: The Planning Commission would need to include findings (see the section above) that the proposed Variance would not have an adverse impact on other properties and the general area or that potential impacts would be mitigated through specific conditions. Additionally, the Commission would need to include findings for the Variances to consider whether there is practical difficulty that would apply.
- (Step 2) Recommendation for Approval: Approval of the **Variances** of 12.5 feet for a front yard setback for the building, and 20-feet for the parking lot setback for the property located at 501 Malden Street, subject to the following conditions:

1) **Compliance with Plan Submittals.** The site shall be utilized in substantial conformance, in the reasonable opinion of the City Council, with the application, narratives, and with the following plans on file with the Engineering Department:

- a) **Application / Narrative (Pope Architects)** dated 08/08/2016
- b) **Site, Building and Landscape Plans (Pope/Loucks)** dated 08/08/2016

2) **Building Permits Required.** Building permits are required for the proposed improvements. All building plans and specifications are subject to the review and approval of the City Building Official and South Metro Fire Marshal.

3) **Parking and ADA Accessible Parking.** The applicant shall stripe the parking spaces and shall sign and stripe the parking lot to provide ADA accessible parking spaces at the building site.

4) **Revised Landscaping Plans.** The applicant shall submit revised landscaping plans to ensure that the landscaping does not encroach on the corner visibility area.

5) **Screening.** Dumpsters shall be screened in accordance with City Code, such screening is subject to the review and approval of the City Planner.

6) **Remove Exterior Storage.** The applicant shall remove the exterior storage just west of the existing building, east of the drive lane, and north of the proposed addition.

7) **Compliance with Laws and Approvals.** The property must remain compliant with all federal, state, and local laws and ordinances and all prior City approvals.

8) **Termination of the Variances.** The Variance will terminate if improvements have not substantially begun within 1-year from the date of approval of the Variances. The violation of any condition of approval shall terminate the Variances.

B. **Denial.** If the Planning Commission does not favor the proposed application or portions thereof, the above requested should be recommended for denial. If the Planning Commission recommends denial, then findings of the basis for denial should be given.

- **Recommendation for Denial:** Denial of the proposed **Variances** for the property located at 501 Malden Street for the following reasons:

1) \_\_\_\_\_

**STAFF RECOMMENDATION**

Staff recommends **approval** of the proposed Variances for the property located at 501 Malden Street subject to the conditions listed in this report.

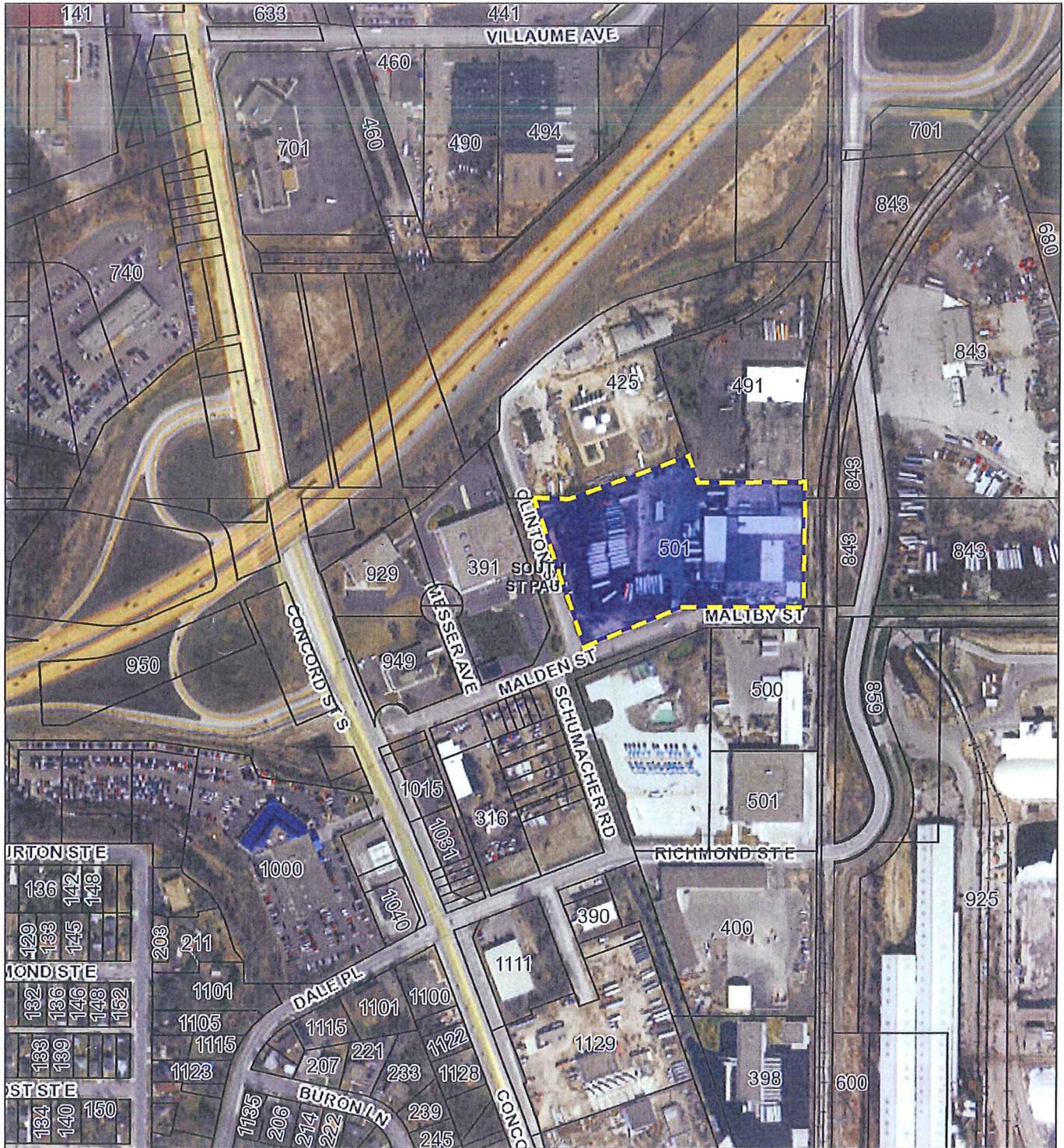
Respectfully Submitted,

*Peter Hellegers*

Peter Hellegers, City Planner

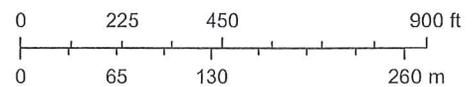
A,

# 501 Malden St



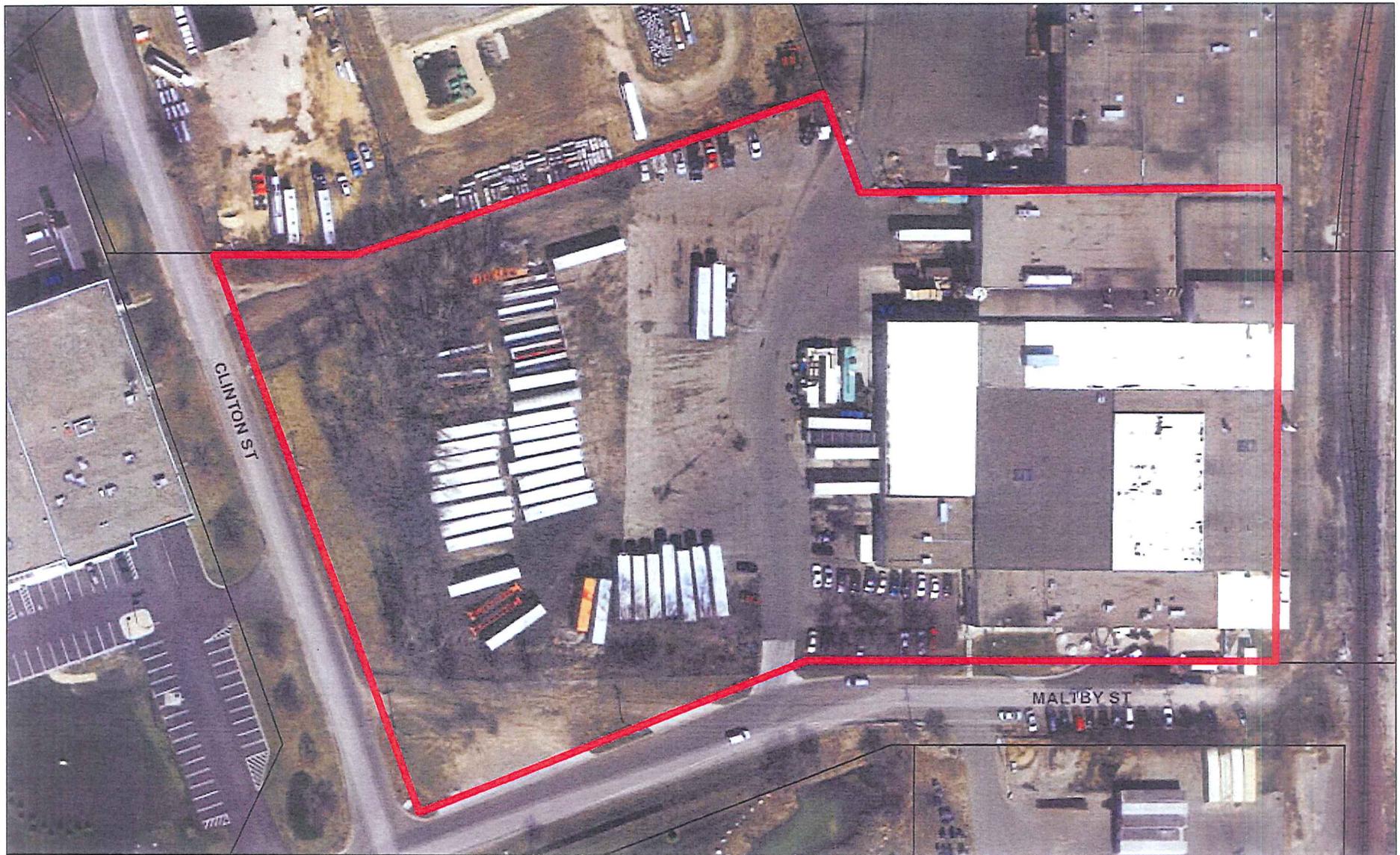
August 19, 2016

1:4,800

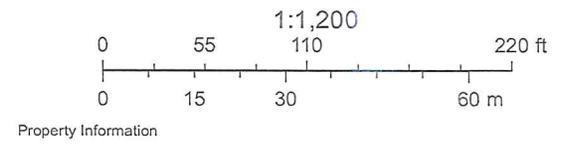


Property Information  
Political Administrative

# 501 Malden St



September 1, 2016



# 501 Malden St



03/15/2015

A3

August 08, 2016

C<sub>1</sub>

City of South St. Paul

125 Third Avenue North

South St. Paul, MN 55075

Re: S.B. Foot Tanning Co. Processing Addition Variance Request

S.B. Foot Tanning is requesting a building setback variance for their new 6,254 sq. ft. collagen processing addition. The proposed addition is to be built adjacent to the southwest corner of the existing building along Maltby / Malden Street to the south. The project will also include a new entrance vestibule for the existing offices, and replacement of employee parking that will be lost with the new addition.

The existing building is approximately 27.3' from the south property line along Maltby / Malden Street. The owner is requesting that the new addition be granted a variance for a 17.6' setback (a difference of 2.4' from the existing setback) from the south property line, along the length of the new addition.

We believe the proposed variance is consistent with the standard for granting a variance, as outlined in City Code section 118-39. The variance is in harmony with the general purpose and intent of the ordinance.

The terms of the variance are consistent with the Comprehensive Plan; the use of the new addition will enhance & improve the existing industrial use on the site, and represents the first of many future enhancements planned for this facility.

The existing building and site have created practical difficulties in complying with the ordinance;

1. The addition is located in the only logical place to support & expand the owner's existing activities; we have reviewed many other options, but without this addition the owner will lose this key business growth opportunity to a competitor somewhere outside of Minnesota.
2. The collagen processing activities that will occur within the addition have an internal clearance requirement that will create a building that is taller than the existing facility. The height difference will create significant snow loading on the existing structure, which would require extensive alterations to the existing building that are cost prohibitive, and would also disrupt the owner's current operations significantly.
3. The collagen processing addition footprint is situated on the site to maximize the efficiency of the owner's business.

The requested setback variance does not alter the essential character of the neighborhood, and in face the new building addition will serve to improve the aesthetics of the facility as well as provide improvements to the site that will be a benefit to the City and the owner.

**POPE ARCHITECTS, INC.**

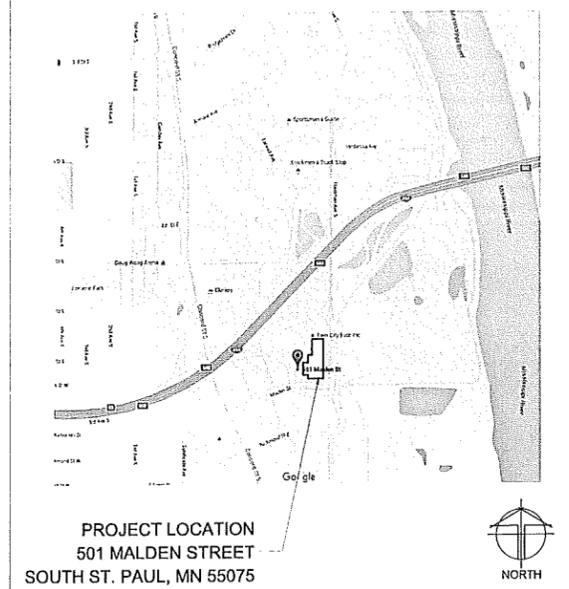
# TWIN CITY TANNING PROCESSING ADDITION & PARKING REPLACEMENT

SITE PLAN SUBMITTAL  
501 MALDEN STREET  
SOUTH ST. PAUL, MN 55075

## SHEET INDEX

SHEET NUMBER	SHEET NAME	07/05/16 CITY SITE PLAN SUBMITTAL	08/08/16 SUBMITTAL REVISION 1
<b>GENERAL</b>			
A0.1	TITLE SHEET	•	•
A0.2	CODE DATA	•	•
<b>SURVEY</b>			
1 of 2	ALTANSPS LAND TITLE SURVEY	•	•
2 of 2	ALTANSPS LAND TITLE SURVEY	•	•
<b>CIVIL &amp; LANDSCAPE</b>			
C1-1	EXISTING CONDITIONS PLAN	•	•
C1-2	DEMOLITION PLAN	•	•
C2-1	SITE PLAN	•	•
C3-1	GRADING, DRAINAGE & EROSION CONTROL PLAN	•	•
C4-1	UTILITY PLAN	•	•
C8-1	CIVIL DETAILS	•	•
L1-1	LANDSCAPE PLAN	•	•
<b>ARCHITECTURAL</b>			
A1.1	SITE PLAN - PHASE I	•	•
A1.2	SITE PLAN - PHASE II (FOR REFERENCE ONLY)	•	•
A2.0	FIRST LEVEL DEMOLITION PLAN	•	•
A2.1	FIRST LEVEL FLOOR PLAN	•	•
A2.2	MEZZANINE LEVEL FLOOR PLAN	•	•
A3.1	EXTERIOR ELEVATIONS	•	•
A4.1	ROOF PLAN	•	•
A5.1	BUILDING & WALL SECTIONS	•	•
A5.2	WALL SECTIONS	•	•
A5.3	WALL SECTION & DETAILS	•	•
A7.1	OPENING SCHEDULE, INTERIOR FINISHES & DETAILS	•	•
A9.1	REFLECTED CEILING PLAN	•	•

## VICINITY MAP, NTS



## PROJECT DIRECTORY

### OWNER

S.B. Foot Tanning Co.  
805 Bench Street  
Red Wing, MN 55066  
Phone: (651) 385-5224  
Contact: Bob Hawley  
Email: bob.hawley@redwingshoes.com

### GENERAL CONTRACTOR

Gunderson Construction, Inc.  
1760 Commerce Court  
White Bear Lake, MN 55110  
Phone: (651) 770-5001  
Contact: Dick Gunderson  
Email: dickg@gundersonconstruction.com

### CIVIL & LANDSCAPE

Loucks  
7200 Hemlock Lane  
Suite 300  
Maple Grove, MN 55369  
Phone: (763) 424-5505  
Contact: Mike St. Martin  
Email: mstmartin@loucksinc.com

### ARCHITECT

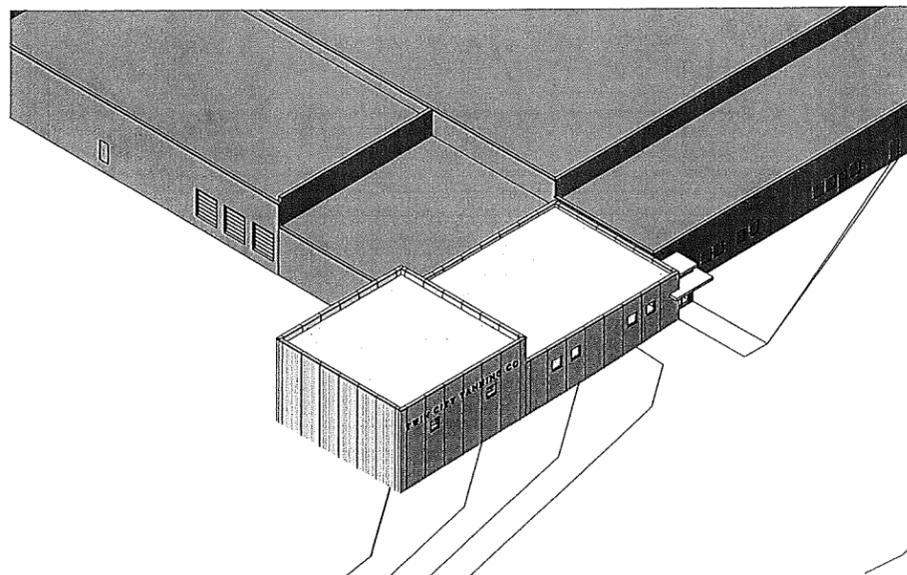
Pope Architects, Inc.  
1295 Bandana Boulevard North  
Suite 200  
St. Paul, MN 55108-2735  
Phone: (651) 789-1643  
Contact: Matthew Anderson  
Email: manderson@popearch.com

### STRUCTURAL

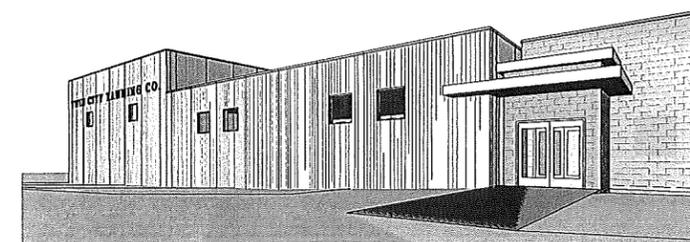
Clark Engineering Corporation  
621 Lilac Drive North  
Minneapolis, MN 55422  
Phone: (763) 545-9196  
Contact: Cory Casperson  
Email: ccasperson@clark-eng.com

### CODE CONSULTANT

Summit Companies  
575 Minnehaha Avenue West  
St. Paul, MN 55103  
Phone: (651) 251-1872  
Contact: Chris Leaver  
Email: cleaver@summitcous.com



2 AERIAL VIEW FROM SOUTHWEST

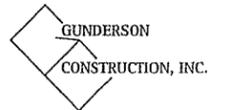


1 PERSPECTIVE - NEW OFFICE VESTIBULE FROM SOUTHEAST

NOT FOR CONSTRUCTION - FOR REFERENCE ONLY



POPE ARCHITECTS, INC.  
1295 BANDANA BLVD. N, SUITE 200  
ST. PAUL, MN 55108-2735  
(651) 642-9200 | FAX (651) 642-1101  
www.popearch.com



TWIN CITY TANNING  
PROCESSING ADDITION  
SOUTH ST. PAUL, MN

### TITLE SHEET

DATE	07/05/16
1 CITY SUBMITTAL	07/05/16
2 REVISION 1	08/08/16

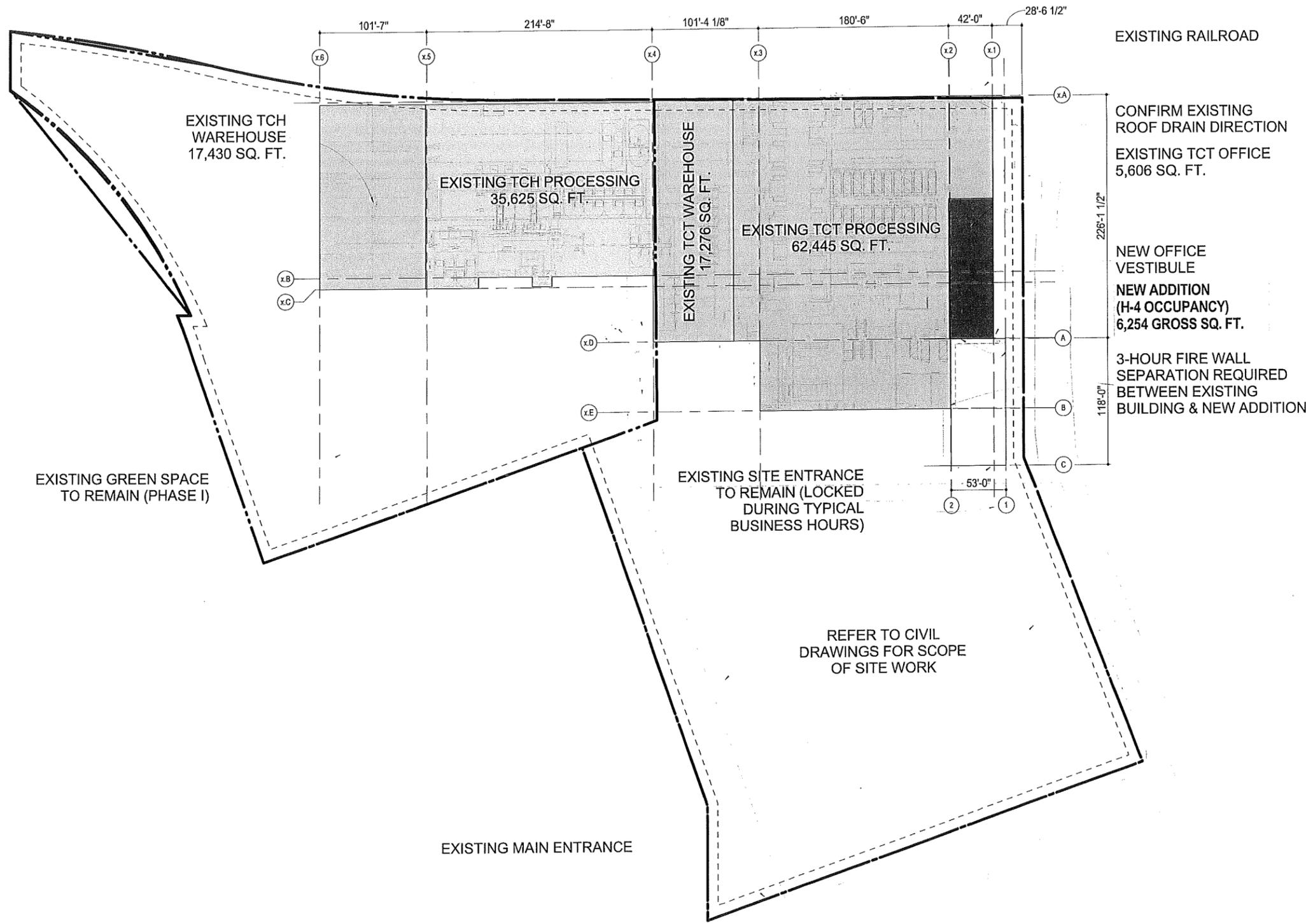
37522-16073  
MHA

SHEET



**SITE PLAN GENERAL NOTES**

- A) SITE INFORMATION HAS BEEN TAKEN FROM A SITE SURVEY PREPARED BY LOUCKS DATED 04/13/2016. GENERAL CONTRACTOR IS TO VERIFY ALL SITE INFORMATION BEFORE STARTING CONSTRUCTION, AND NOTIFY THE ARCHITECT IN WRITING OF ANY DISCREPANCIES.
- B) REFER TO THE GENERAL NOTES FOR OTHER APPLICABLE INFORMATION.



EXISTING RAILROAD

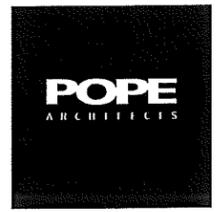
CONFIRM EXISTING ROOF DRAIN DIRECTION

EXISTING TCT OFFICE  
5,606 SQ. FT.

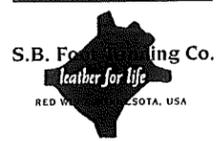
NEW OFFICE VESTIBULE  
NEW ADDITION  
(H-4 OCCUPANCY)  
6,254 GROSS SQ. FT.

3-HOUR FIRE WALL SEPARATION REQUIRED BETWEEN EXISTING BUILDING & NEW ADDITION

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TWIN CITY TANNING  
PROCESSING ADDITION  
SOUTH ST. PAUL, MN

**SITE PLAN - PHASE I**

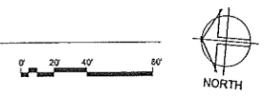
NO.	DESCRIPTION	DATE
1	CITY SUBMITTAL	07/05/16
2	REVISION 1	08/08/16

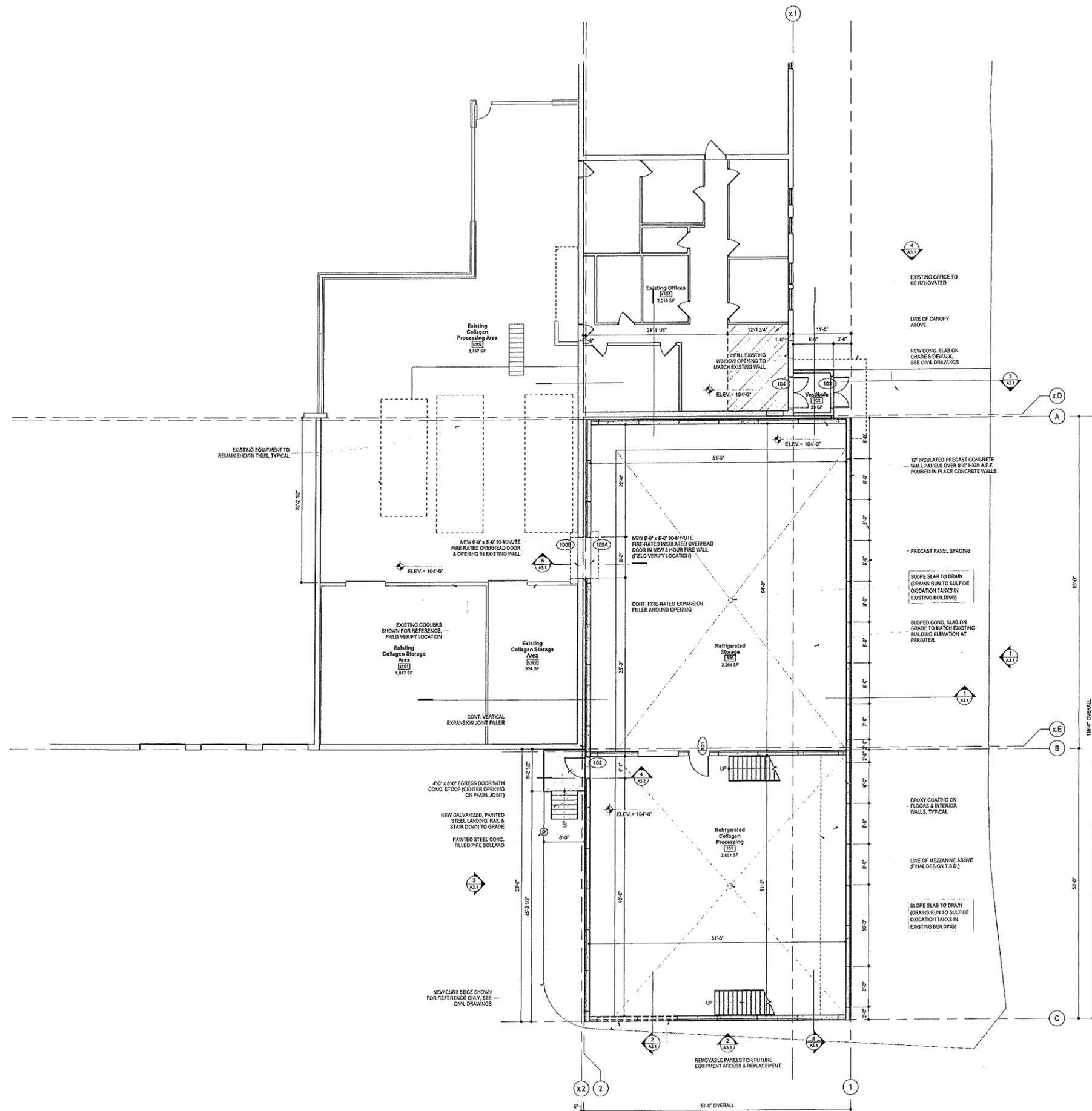
Project No: 37522-16073  
Company: MHA  
Created by:

SHEET

**A1.1**

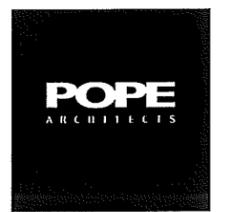
TRUE SHEET SCALE



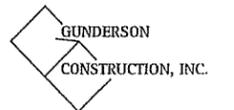
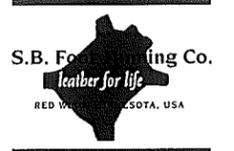


- ### FLOOR PLAN GENERAL NOTES
- A) DO NOT SCALE DRAWINGS.
  - B) NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES.
  - C) IN THE CASE OF AMBIGUITIES, DISCREPANCIES, OR IRREGULARITIES IN THE DRAWINGS AND/OR SPECIFICATIONS, THE CONTRACTOR SHALL SUBMIT A WRITTEN REQUEST FOR CLARIFICATION FROM THE ARCHITECT BEFORE PROCEEDING WITH THE WORK.
  - D) IN ALL CONSTRUCTION TYPES, ALL WOOD USED IN THE FOLLOWING LOCATIONS IS TO BE PRESERVATIVE-TREATED:
    1. WOOD IN CONTACT WITH THE GROUND OR WATER.
    2. WOOD IN EXTERIOR FOUNDATION WALLS.
    3. WOOD IN CONTACT WITH CONCRETE SLABS ON GRADE, OR IN CONTACT WITH CONCRETE OR MASONRY FOUNDATION WALLS.
    4. WOOD WITHIN A CRAVA, SPACE OVER EXPOSED EARTH.
    5. AT OTHER LOCATIONS NOTED ON THE CONSTRUCTION DOCUMENTS.
  - E) ALL WOOD USED IN BUILDINGS OF TYPE I OR II CONSTRUCTION (SEE CODE DATA SHEET), IS TO BE FIRE-RETARDANT-TREATED, WITH THE FOLLOWING EXCEPTIONS:
    1. PRESERVATIVE-TREATED WOOD AS NOTED IN GENERAL NOTE 'D' ABOVE.
    2. INTERIOR FLOOR FINISH AND INTERIOR FINISHES; TRIM AND MILLWORK SUCH AS CABINETS, DOORS, DOOR FRAMES AND WINDOWS.
    3. BLOCKING FOR HANDRAILS, MILLWORK, CABINETS, AND WINDOW AND DOOR FRAMES.
  - F) SEE SHEET A2.2 FOR SYMBOLS, INDICATION OF MATERIALS, PARTITION TYPE DETAILS & ABBREVIATIONS.

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TWIN CITY TANNING  
PROCESSING ADDITION  
SOUTH ST. PAUL, MN

### FIRST LEVEL FLOOR PLAN

NO.	DESCRIPTION	DATE
1	CITY SUBMITTAL	07/05/16
2	REVISION 1	08/08/16

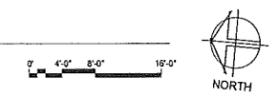
Project No: 37522-16073  
Owner: MHA  
Architect: POPE ARCHITECTS

SHEET

**A2.1**

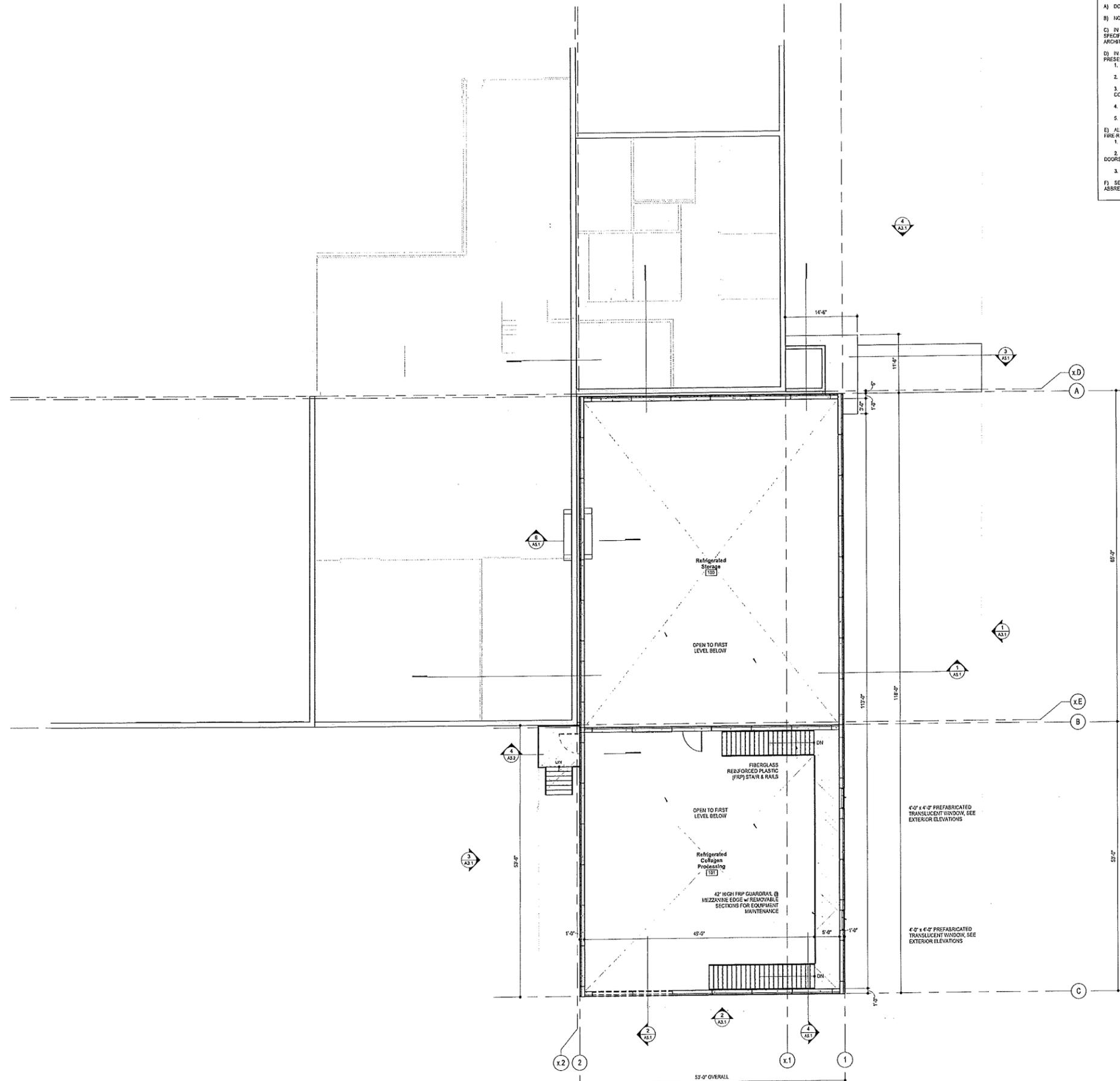
SCALE SHEET

1 FIRST LEVEL FLOOR PLAN  
A2.1 1/8" = 1'-0"



**FLOOR PLAN GENERAL NOTES**

- A) DO NOT SCALE DRAWINGS.
- B) NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES.
- C) IN THE CASE OF AMBIGUITIES, DISCREPANCIES, OR IRREGULARITIES IN THE DRAWINGS AND/OR SPECIFICATIONS, THE CONTRACTOR SHALL SUBMIT A WRITTEN REQUEST FOR CLARIFICATION FROM THE ARCHITECT BEFORE PROCEEDING WITH THE WORK.
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  - 3. WOOD IN CONTACT WITH CONCRETE SLABS-ON-GRADE, OR IN CONTACT WITH CONCRETE OR MASONRY FOUNDATION WALLS.
  - 4. WOOD WITHIN A CRAWL SPACE OVER EXPOSED EARTH.
  - 5. AT OTHER LOCATIONS NOTED ON THE CONSTRUCTION DOCUMENTS.
- E) ALL WOOD USED IN BUILDINGS OF TYPE I OR II CONSTRUCTION (SEE CODE DATA SHEET), IS TO BE FIRE-RETARDANT-TREATED, WITH THE FOLLOWING EXCEPTIONS:
  - 1. PRESERVATIVE-TREATED WOOD AS NOTED IN GENERAL NOTE "D" ABOVE.
  - 2. INTERIOR FLOOR FINISH AND INTERIOR FINISHES, TRIM AND MILLWORK SUCH AS CABINETS, DOORS, DOOR FRAMES AND WINDOWS.
  - 3. BLOCKING FOR HANDRAILS, MILLWORK, CABINETS, AND WINDOW AND DOOR FRAMES.
- F) SEE SHEET A2.2 FOR SYMBOLS, INDICATION OF MATERIALS, PARTITION TYPE DETAILS & ABBREVIATIONS.



NOT FOR CONSTRUCTION - FOR REFERENCE ONLY



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TWIN CITY TANNING  
PROCESSING ADDITION  
SOUTH ST. PAUL, MN

**MEZZANINE LEVEL FLOOR PLAN**

1	CITY SUBMITTAL	07/05/16
2	REVISION 1	08/08/16

Project No: 37522-16073  
Client: MHA  
Contractor:

SHEET

**A2.2**

1/8" = 1'-0"

0" 4'-0" 8'-0" 12'-0"

NORTH

**1 MEZZANINE LEVEL FLOOR PLAN**  
1/8" = 1'-0"

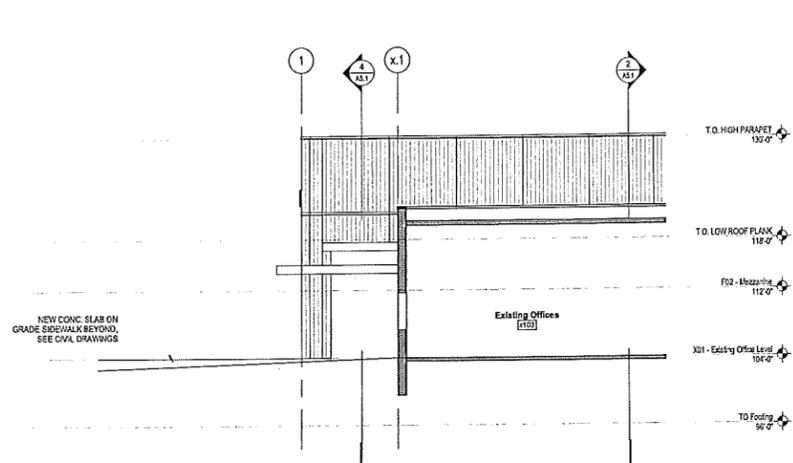


POPE ARCHITECTS, INC.  
 1285 BANDANA BLVD N, SUITE 200  
 ST. PAUL, MN 55108-2735  
 (651) 647-9200 | FAX (651) 643-1101  
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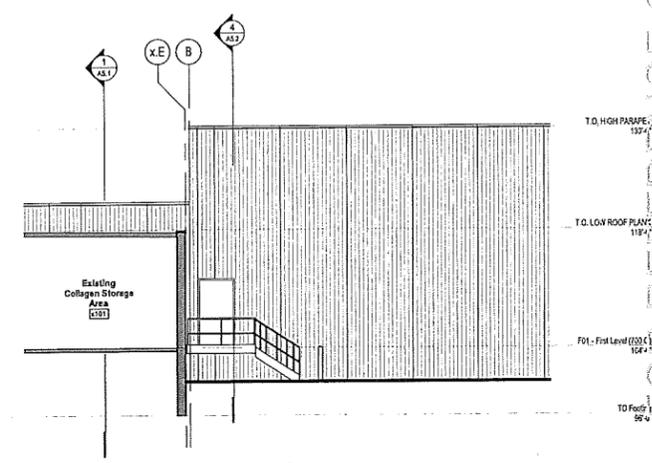


TWIN CITY TANNING  
 PROCESSING ADDITION  
 SOUTH ST. PAUL, MN

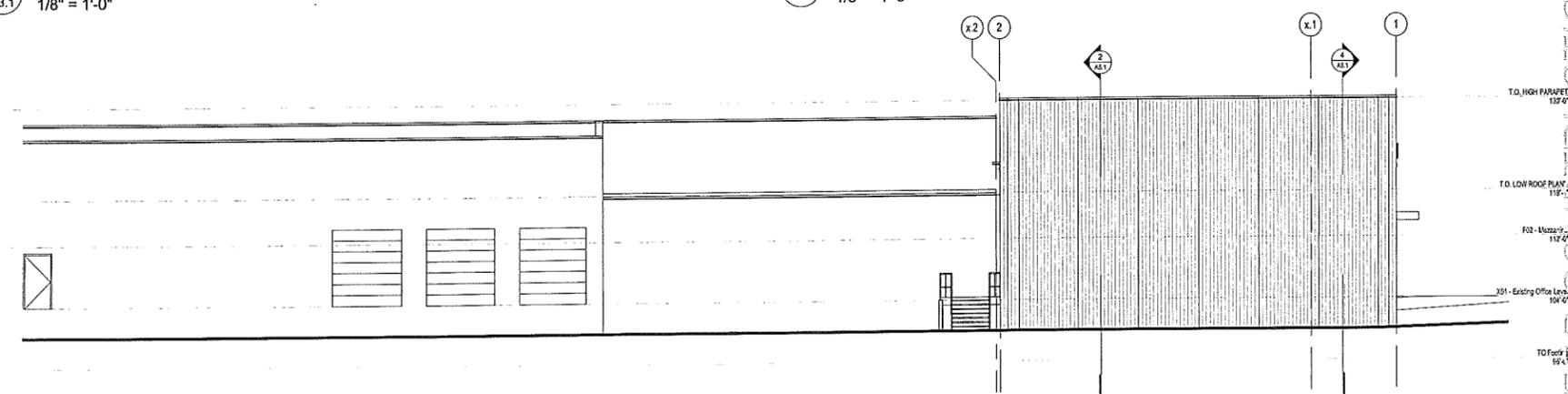
NOT FOR CONSTRUCTION - FOR REFERENCE ONLY



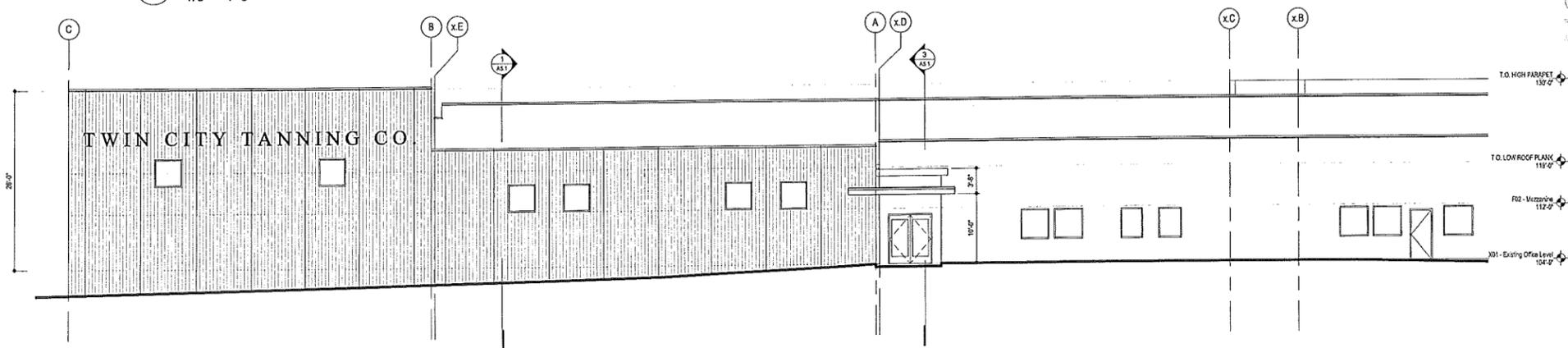
4 EAST EXTERIOR ELEVATION / SECTION  
 A3.1 1/8" = 1'-0"



3 NORTH EXTERIOR ELEVATION / SECTION  
 A3.1 1/8" = 1'-0"



2 WEST EXTERIOR ELEVATION  
 A3.1 1/8" = 1'-0"



1 SOUTH EXTERIOR ELEVATION  
 A3.1 1/8" = 1'-0"

EXTERIOR ELEVATIONS

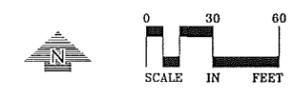
DATE	DESCRIPTION
07/05/16	1 CITY SUBMITTAL
08/08/16	2 REVISION 1

37522-16073  
 MHA

SHEET

**A3.1**

THE SHEET SCALE



**NOT FOR CONSTRUCTION - FOR REFERENCE ONLY**



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ST. PAUL, MN 55108-2735  
(651) 642-9200 | FAX (651) 642-1101  
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**S.B. FOOT TANNING CO.**  
PROCESSING ADDITION  
SOUTH ST. PAUL, MN

**EXISTING CONDITIONS PLAN**

DATE	DESCRIPTION
07/05/16	1. CITY SUBMITTAL
07/26/16	2. CITY SUBMITTAL
08/08/16	3. CITY SUBMITTAL

I hereby certify that the plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.

Version: 16.000-16  
License No. 24440  
Date

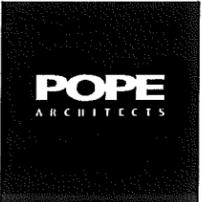


**CALL BEFORE YOU DIG!**  
**Gopher State One Call**  
TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-522-1166

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PROJECT: 71255-16073  
DATE: DDL  
DRAWN BY: MUS

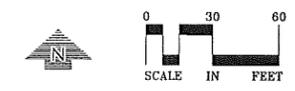
SHEET  
**C1-1**  
FILE SHEET SCALE  
0" = 10' 0" 1" = 10' 0" 1" = 10' 0"



POPE ARCHITECTS, INC.  
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PROCESSING ADDITION  
SOUTH ST. PAUL, MN



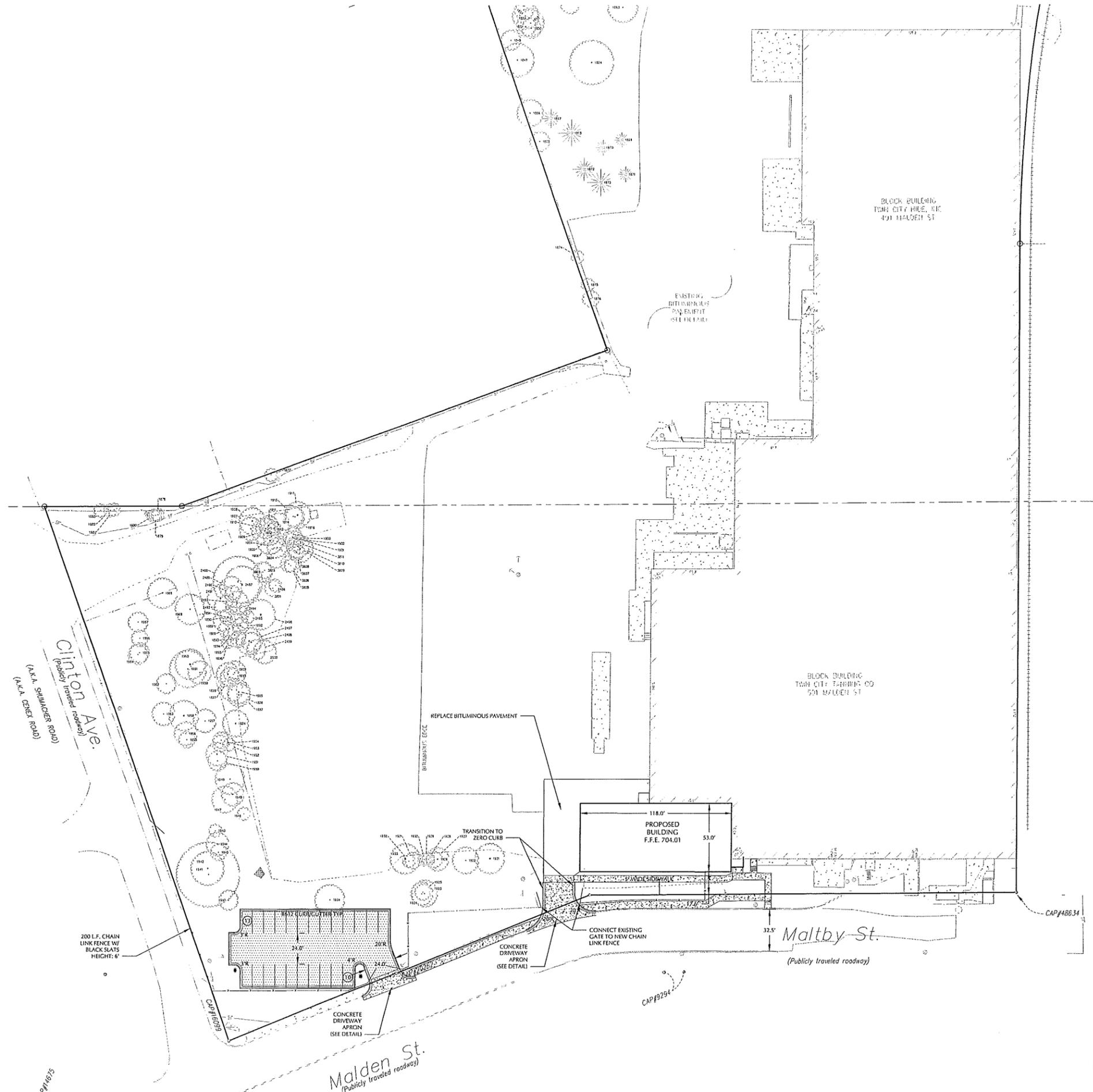
EXISTING	CIVIL LEGEND	PROPOSED
○	SANITARY MANHOLE	○
○	STORM MANHOLE	○
○	CATCH BASIN	○
○	VALVE	○
○	WATERLINE	○
○	POLE INDICATOR VALVE	○
○	LIGHT POLE	○
○	POWER POLE	○
○	SOIL	○
○	BENCH MARK	○
○	WATER MANHOLE	○
○	TELEPHONE MANHOLE	○
○	UTILITY MANHOLE	○
○	ELECTRIC MANHOLE	○
○	WATER SERVICE	○
○	WINDUP PARALLEL	○
○	DIRECTION OF FLOW	○
○	SPOT ELEVATION	○
○	CONTOURS	○
○	SANITARY SEWER	○
○	STORM SEWER	○
○	WATERMAIN	○
○	TERRACING	○
○	GRANITE	○
○	SLT FENCE	○
○	CURB & GUTTER	○
○	RETAINING WALL	○
○	TREELINE	○
○	EXISTING LINE	○
○	SEARCH LINE	○
○	FENCE LINE	○
○	UNDERGROUND TILE	○
○	UNDERGROUND GAS	○
○	OVERHEAD UTILITY	○

**SITE PLAN GENERAL NOTES**

- SETBACKS:**  
PARKING: FRONT: 20' SIDE/REAR: 10'  
BUILDING: FRONT: 30' SIDE/REAR: 20' SIDE/REAR ON RAILROAD: 0'
- ZONING:**  
EXISTING ZONING = INDUSTRIAL
- PARKING SUMMARY:**  
STANDARD CAR PARKING STALLS = 58 (2 HANDICAPPED)  
TRUCK PARKING STALLS = 41 (5 TANDEM)
- AREA/DENSITY:**  
EXISTING IMPERVIOUS SURFACE AREA = 4.17 AC.  
EXISTING PERVIOUS SURFACE AREA = 2.02 AC.  
PROPOSED IMPERVIOUS SURFACE AREA = 4.62 AC.  
PROPOSED PERVIOUS SURFACE AREA = 1.57 AC.
- ALL PAVING, CONCRETE CURB, GUTTER AND SIDEWALK SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE DETAILS SHOWN PER SHEET C8-1 AND THE REQUIREMENTS OF THE CITY. SEE LANDSCAPE AND ARCHITECTURAL PLANS FOR ANY ADDITIONAL HARDSCAPE APPLICATIONS.
- THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPT. AND THE CONSTRUCTION ENGINEER SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE STREET RIGHT OF WAY (SIDEWALK, STREET OR DRIVEWAYS).
- MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER 'GOPHER STATE ONE CALL' PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK.
- SEE CONTRACT SPECIFICATIONS FOR ANY REMOVAL DETAILS.
- ANY SIGN OR FIXTURES REMOVED WITHIN THE RIGHT OF WAY OR AS PART OF THE SITE WORK SHALL BE REPLACED BY THE CONTRACTOR IN ACCORDANCE WITH THE CITY REQUIREMENTS. THE CONTRACTOR SHALL PRESERVE AND MAINTAIN ANY EXISTING STREET LIGHTS AND TRAFFIC SIGNS PER THE REQUIREMENTS OF THE CITY.
- CLEAR AND GRUB AND REMOVE ALL TREES, VEGETATION AND SITE DEBRIS PRIOR TO GRADING. ALL REMOVED MATERIAL SHALL BE HAULED FROM THE SITE DAILY. ALL CLEARING AND GRUBBING AND REMOVALS SHALL BE PERFORMED PER THE CONTRACT SPECIFICATIONS. EROSION CONTROL MEASURES SHALL BE IMMEDIATELY ESTABLISHED UPON REMOVAL. (SEE SHEET C3-1)
- THE CONTRACTOR SHALL BE REQUIRED TO OBTAIN ALL PERMITS FROM THE CITY AS REQUIRED FOR ALL WORK WITHIN THE STREET AND PUBLIC RIGHT OF WAY.
- THE CONTRACTOR SHALL SAW-CUT BITUMINOUS AND CONCRETE PAVEMENTS AS REQUIRED PER THE SPECIFICATIONS.
- SEE SHEETS C3-1 AND C4-1 FOR GRADING AND UTILITIES.
- ALL CONCRETE CURB AND GUTTER B612, CITY'S STANDARD PLATES. (SEE DETAIL SHEETS).

**NOT FOR CONSTRUCTION - FOR REFERENCE ONLY**

Chicago & Northwestern Railway



**SITE PLAN**

REV	DATE	DESCRIPTION
1	07/05/16	CITY SUBMITTAL
2	07/26/16	CITY SUBMITTAL
3	08/08/16	CITY SUBMITTAL

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

License No. 26440  
Date

71255-16073  
DDL  
MJS

SHEET

**C2-1**



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CAP#1875

200 L.F. CHAIN LINK FENCE W/ BLACK SLATS HEIGHT: 6'

Malden St.  
(Publicly traveled roadway)

Maltby St.  
(Publicly traveled roadway)

CAP#294

CAP#48634



POPE ARCHITECTS, INC.  
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SOUTH ST. PAUL, MN

GRADING, DRAINAGE & EROSION CONTROL PLAN

NO.	DATE	DESCRIPTION
1	07/05/16	CITY SUBMITTAL
2	07/26/16	CITY SUBMITTAL
3	08/08/16	CITY SUBMITTAL

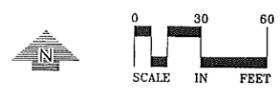
I hereby certify that the plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Michael St. John - PE License No. 24440 Date

Checked by: 71255-16073  
Drawn by: DDL  
Title block by: MJS

SHEET

C3-1



EXISTING	CIVIL LEGEND	PROPOSED
	SANITARY MANHOLE	
	STORM MANHOLE	
	CATCH BASIN	
	CULVERT	
	INLET	
	GATE VALVE	
	POST POSITION VALVE	
	LIGHT POLE	
	FIRE POLE	
	SIGN	
	BENCHMARK	
	SOIL BORING	
	WATER MANHOLE	
	TELEPHONE MANHOLE	
	UTILITY MANHOLE	
	ELECTRIC MANHOLE	
	WATER SERVICE	
	SANITARY SERVICE	
	MANHOLE PAVING	
	DIRECTION OF FLOW	
	SPOT ELEVATION	
	CONTOURS	
	SANITARY SEWER	
	STORM SEWER	
	WATERMAIN	
	FIREMAIN	
	GASLINE	
	SILT FENCE	
	DIKE & OUTLET	
	RETAINING WALL	
	TREEBANK	
	EROSION LINE	
	SECTION LINE	
	FENCE LINE	
	UNDERGROUND TILE	
	UNDERGROUND GAS	
	OVERHEAD UTILITY	

GRADING, DRAINAGE & EROSION CONTROL NOTES

1. THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF BUILDINGS, VESTIBULES, SLOPED PAVING, EXIT PORCHES, RAMPS, TRUCK DOCKS, ENTRY LOCATIONS AND LOCATIONS OF DOWNSPOUTS.
2. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 3 INCHES OF TOP SOIL AND SOO OR SEED. THESE AREAS SHALL BE WATERED BY THE CONTRACTOR UNTIL THE SOO OR SEED IS GROWING IN A HEALTHY MANNER.
3. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGES TO ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASE OF THIS PROJECT.
4. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
5. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING THE PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
6. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTORS PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASURES IN, OR NEAR THE CONSTRUCTION SITE.
7. BEFORE BEGINNING CONSTRUCTION THE CONTRACTOR SHALL INSTALL A TEMPORARY ROCK ENTRANCE PAD AT ALL POINTS OF VEHICLE EXIT FROM THE PROJECT SITE. SAID ROCK ENTRANCE PAD SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF THE PROJECT. SEE DETAILS.
8. EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE ESTABLISHED AROUND THE ENTIRE SITE PERIMETER AND IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, CITY REQUIREMENTS AND THE DETAILS.
9. ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED BY THE CITY.
10. SEE UTILITY PLAN AND STORM SEWER PROFILES FOR FURTHER DETAIL REGARDING THE STORM SEWER.

Chicago & Northwestern Railway



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NOT FOR CONSTRUCTION - FOR REFERENCE ONLY

CAP#14873

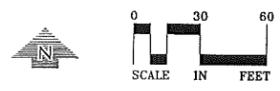
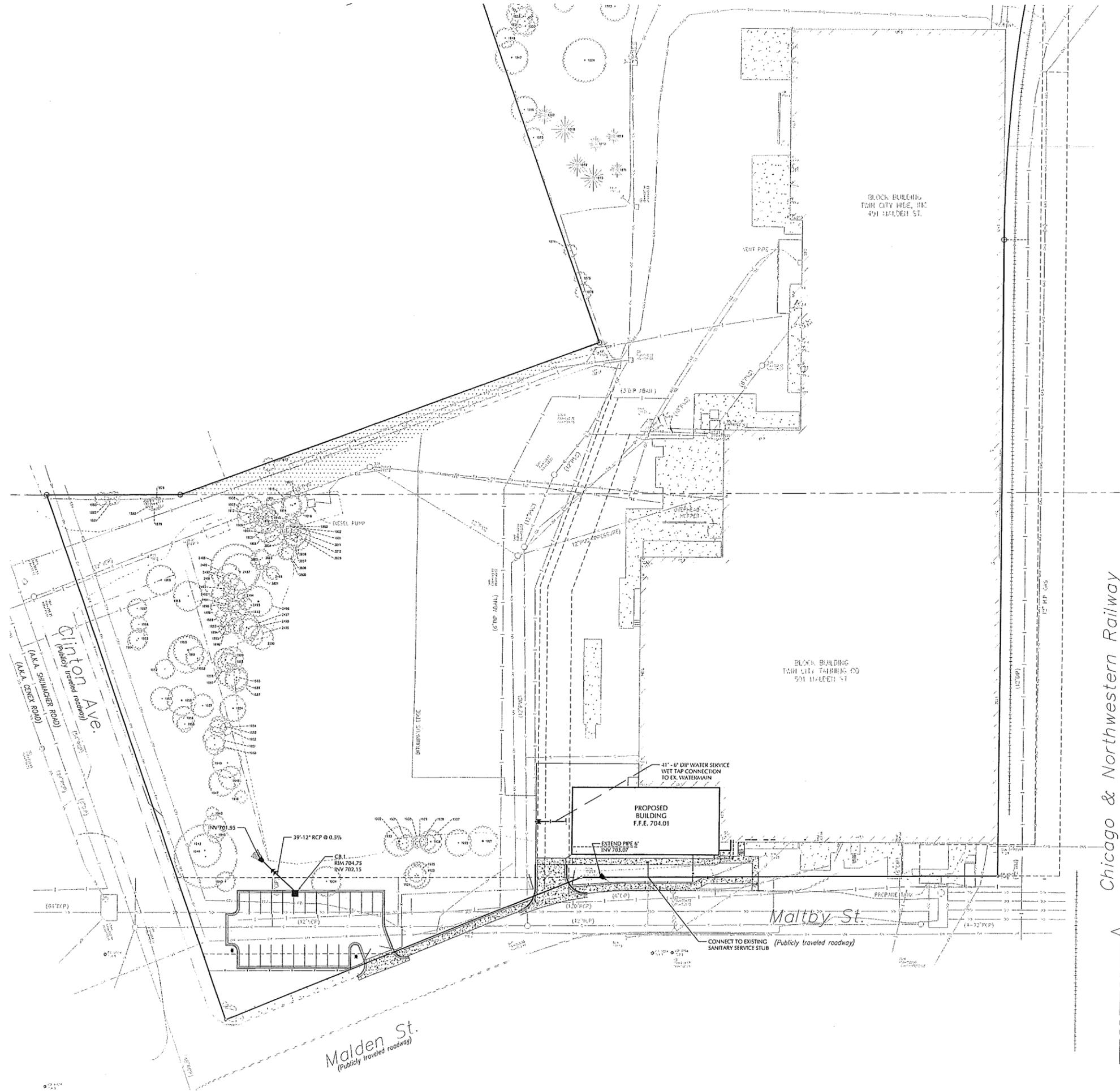
Malden St.  
(Publicly traveled roadway)

F.F.E. 704.10

Maltby St.  
(Publicly traveled roadway)

CAP#148634

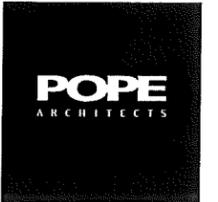
CAP#9254



EXISTING	CIVIL LEGEND	PROPOSED
○	SANITARY MANHOLE	○
○	STORM MANHOLE	○
○	CATCH BASIN	○
○	CULVERT	○
○	INVERT	○
○	SAFETY VALVE	○
○	POLE INDICATOR VALVE	○
○	LIGHT POLE	○
○	POWER POLE	○
○	SON	○
○	BENCHMARK	○
○	BOX MANHOLE	○
○	WATER MANHOLE	○
○	TELEPHONE MANHOLE	○
○	UTILITY MANHOLE	○
○	ELECTRIC MANHOLE	○
○	WATER SERVICE	○
○	SANITARY SERVICE	○
○	MANHOLE FINISH	○
○	DIRECTION OF FLOW	○
○	SPOT ELEVATION	○
○	CONTOURS	○
○	SANITARY SEWER	○
○	STORM SEWER	○
○	WATERWORK	○
○	FOREDRAIN	○
○	SHANTILE	○
○	SILT FENCE	○
○	CLIP & GUTTER	○
○	RETAINING WALL	○
○	TRAILING	○
○	EXTEND LINE	○
○	SEARCH LINE	○
○	FENCE LINE	○
○	UNDERGROUND TIE	○
○	UNDERGROUND GAS	○
○	OVERHEAD UTILITY	○

- UTILITY NOTES**
1. ALL STORM SEWER SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE MANUFACTURER SPECIFICATIONS, THE MINNESOTA PLUMBING CODE, THE LOCAL GOVERNING UNIT, AND THE STANDARD UTILITIES SPECIFICATION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 2013 EDITION.
  2. ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL. ALL CONSTRUCTION SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CEAM SPECIFICATION.
  3. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
  4. ALL PORTIONS OF THE STORM SEWER SYSTEM, INCLUDING CATCH BASINS, LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED ACCORDANCE WITH MINNESOTA RULES, PART 4715.2620

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PROCESSING ADDITION  
SOUTH ST. PAUL, MN

UTILITY PLAN

NO. OF SHEETS	CITY SUBMITTAL	DATE
1	CITY SUBMITTAL	07/05/16
2	CITY SUBMITTAL	07/26/16
3	CITY SUBMITTAL	08/08/16

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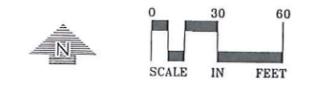
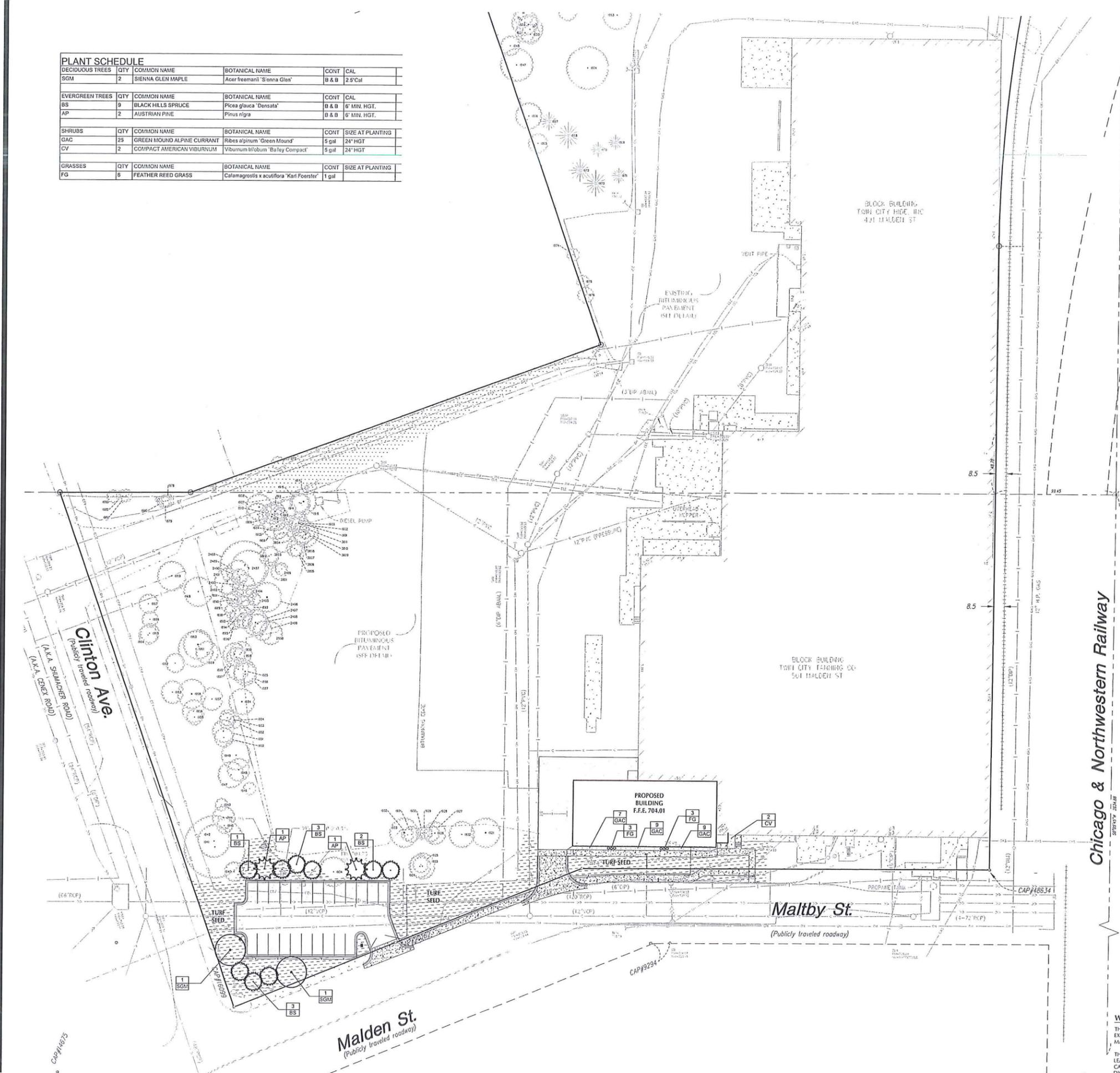
Contract No. 16-01-2440  
Date

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SHEET  
**C4-1**  
SCALE: AS SHOWN  
DATE: 08/08/16

PLANT SCHEDULE					
<b>DECIDUOUS TREES</b>					
QTY	COMMON NAME	BOTANICAL NAME	CONT	CAL	
2	SIENNA GLEN MAPLE	Acer freemanii 'Sienna Glen'	B & B	2 5' Cal	
<b>EVERGREEN TREES</b>					
QTY	COMMON NAME	BOTANICAL NAME	CONT	CAL	
9	BLACK HILLS SPRUCE	Picea glauca 'Densata'	B & B	6' MIN. HGT.	
2	AUSTRIAN PINE	Pinus nigra	B & B	6' MIN. HGT.	
<b>SHRUBS</b>					
QTY	COMMON NAME	BOTANICAL NAME	CONT	SIZE AT PLANTING	
25	GREEN MOUND ALPINE CURRANT	Ribes alpinum 'Green Mound'	5 gal	24" HGT	
2	COMPACT AMERICAN VIBURNUM	Viburnum lnt'obum 'Bailey Compact'	5 gal	24" HGT	
<b>GRASSES</b>					
QTY	COMMON NAME	BOTANICAL NAME	CONT	SIZE AT PLANTING	
8	FEATHER REED GRASS	Calamagrostis x acutiflora 'Karl Foerster'	1 gal		



**GENERAL NOTES**

CONTRACTOR SHALL VISIT SITE PRIOR TO SUBMITTING BID. HE SHALL INSPECT SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK.

VERIFY LAYOUT AND ANY DIMENSIONS SHOWN AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT ANY DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN AND/OR INTENT OF THE PROJECT'S LAYOUT.

ASSURE COMPLIANCE WITH ALL APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK OR MATERIALS SUPPLIED.

CONTRACTOR SHALL PROTECT ALL EXISTING ROADS, CURBS/CUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING PLANTING OPERATIONS. ANY DAMAGE TO SAME SHALL BE REPAIRED AT NO COST TO THE OWNER.

CONTRACTOR SHALL VERIFY ALIGNMENT AND LOCATION OF ALL UNDERGROUND AND ABOVE GRADE UTILITIES AND PROVIDE THE NECESSARY PROTECTION FOR SAME BEFORE CONSTRUCTION/MATERIAL INSTALLATION BEGINS (MINIMUM 10'-0" CLEARANCE).

ALL UNDERGROUND UTILITIES SHALL BE LAID SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF ANY EXISTING TREES TO REMAIN.

EXISTING CONTOURS, TRAILS, VEGETATION, CURB/CUTTER AND OTHER EXISTING ELEMENTS BASED UPON INFORMATION SUPPLIED TO LANDSCAPE ARCHITECT BY OTHERS. CONTRACTOR SHALL VERIFY ANY AND ALL DISCREPANCIES PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT OF SAME.

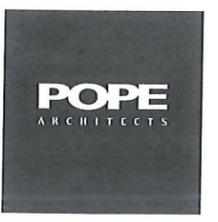
THE ALIGNMENT AND GRADES OF THE PROPOSED WALKS, TRAILS AND/OR ROADWAYS ARE SUBJECT TO FIELD ADJUSTMENT REQUIRED TO CONFORM TO LOCALIZED TOPOGRAPHIC CONDITIONS AND TO MINIMIZE TREE REMOVAL AND GRADING. ANY CHANGE IN ALIGNMENT MUST BE APPROVED BY LANDSCAPE ARCHITECT.

**IRRIGATION/WATERING NOTES:**

IRRIGATION IS NOT PLANNED FOR THIS SITE. FOR THE HEALTH AND SURVIVAL OF ALL PROPOSED PLANTINGS, REGULAR WATERING OF THE PLANTINGS IS RECOMMENDED.

FOR ESTABLISHMENT OF PLANTINGS, THE CONTRACTOR IS TO REGULARLY WATER NEWLY INSTALLED PLANTINGS DURING PROJECT CONSTRUCTION. CONTRACTOR TO PROVIDE OWNER WITH WATERING RECOMMENDATIONS OR WATERING CONTRACT FOR THE 1 YEAR WARRANTY PERIOD.

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SOUTH ST. PAUL, MN

**LANDSCAPE PLAN**

REV	DESCRIPTION	DATE
1	CITY SUBMITTAL	07/05/16
2	CITY SUBMITTAL	07/26/16
3	CITY SUBMITTAL	08/08/16

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.

Designer: Douglas B. Lohrey - LA  
License No. 45591  
Date:

Project No. 71255-16073  
Job No. DDL  
Sheet No. DDL

SHEET

**L1-1**



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Planning Commission Meeting Date:  <b>Wednesday, September 7, 2016</b>	 <b>City of South St. Paul Planning Commission Report</b>	PC Agenda Item:  <b>3.F</b>
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<b>Project Name:</b>	Accessory Building Ordinance Amendments
<b>Request:</b>	Consider an ordinance amending the number of Accessory Buildings
<b>Proceeding:</b>	<ul style="list-style-type: none"> <li>▪ Public Hearing</li> <li>▪ Planning Commission Resolution (Recommendation to City Council)</li> </ul>
<b>City Council Meeting Date(s):</b>	City Council – 1 <sup>st</sup> Reading – September 19, 2016 (tentative) City Council – 2 <sup>nd</sup> Reading – October 3, 2016 (tentative)
<b>Exhibits:</b>	A. PC Resolution 2016-05 – Accessory Buildings Amendment B. Proposed amendments to R-1 District and Accessory Building Standards

**Update**

This item was continued from the August 3<sup>rd</sup> and July 6<sup>th</sup> Planning Commission meetings. The Planning Commission is asked to review the two options to change the regulations pertaining to accessory buildings, weigh those changes against the existing language, and recommend which way to proceed.

**Request**

Review the attached draft ordinance regarding changes to the R-1 Zoning District and Accessory Building Standards. Conduct the public hearing for the draft ordinance and provide a recommendation to the City Council. Staff has prepared Planning Commission Resolution 2016-05 which would recommend that the City Council adopt the proposed ordinance.

**What Would the Proposed Ordinance Do?**

The proposed amendment would:

- Eliminate language stating that a property is limited to one garage (attached or detached) plus one other accessory building and replace it with allowing two accessory buildings. Attached garages would be considered part of the principal building (house) and would not count against number of accessory buildings.

- Existing language capping an accessory garage building to 1,000 square feet and an additional accessory building (shed) at 200 square feet, for a grand total of 1,200 square feet of accessory buildings.

#### Why Should the City Take This Action?

In a recent application a property that had an existing attached garage and existing shed and was requesting to add a detached garage the current standards would have required that the shed be removed to build the new garage. However if the property had not had an attached garage and was looking to build a large detached garage then removal of the shed would not have been an issue. This case highlighted a conflict in the code where homes with attached garages essentially had less flexibility with regard to adding accessory structures to their property. Since there are many properties that have attached 1-car garages and tuck-under garages which do not accommodate today's vehicles the Code would push those properties into requiring a Conditional Use Permit for a second garage. After discussing the case listed above the City Council directed staff to examine ways to remedy the situation so that variances would not be required to have a shed along with the detached garage and attached garage.

In the proposed amendment (shown as A) the language is simplified to state that a property may have 2 accessory buildings while the maximum size for a garage (1,000 s.f.) and the maximum size for the second accessory building/shed (200 s.f.) are retained.

#### Items for additional discussion

##### Alternative Amendment Language

##### Should there be a Cap on the maximum size of garages whether attached or detached?

The proposed amendment was intended to correct the situation where properties with a 1-car/tuck-under attached garage are forced into needing a Conditional Use Permit to build a new detached garage or converting the tuck-under garage into part of the finished space in the house. However, the same language would allow a property that already has a 3-car attached garage to also build 2 accessory buildings totaling up to 1,200 square feet.

1. Is the Planning Commission comfortable with the language as proposed noting the situation listed above could arise? Alternative language (shown as B) would cap the maximum size for the accessory uses (garages, sheds, etc.) at 1,200 square feet whether the accessory use is in a separate building or is attached to the house.

*For example a home with a 768 square foot attached garage would then have the ability to have another 432 square feet of space that could be in 2 accessory buildings (a detached 1-car garage and a small shed or 2 larger sheds)*

The final option of course is to recommend that no action be taken and the language would remain as is.

**Process / Timeline**

Process – The Planning Commission will hold the public hearing, make a recommendation, and the ordinance amendment would be forwarded to the City Council to be considered at two readings of the ordinance.

Timeline - The first reading for this item would be at the August 15th City Council meeting and the 2<sup>nd</sup> reading, would be scheduled for either the September 19<sup>th</sup> City Council meeting. If approved by the Council the ordinance would be in effect from the date of adoption.

**Staff Recommendation**

Open the public hearing and discuss the proposed ordinance options (A or B) and alternatively whether the City Code language should be left as is. Staff recommends approval of PC Resolution No. 2016-05 which would recommend approval of the ordinance (per option A) amending the standards for Accessory Buildings.

Respectfully Submitted,

*Peter Hellegers*

Peter Hellegers, City Planner

City of South St. Paul  
Dakota County, Minnesota

**PLANNING COMMISSION  
RESOLUTION NUMBER 2016-05**

**RECOMMENDING APPROVAL OF AN AMENDMENT TO  
SECTIONS 118-121 AND 118-208 OF THE CITY CODE REGARDING  
REQUIREMENTS FOR ACCESSORY BUILDINGS**

**WHEREAS**, the City Code establishes certain standards for development in the City of South St. Paul, including standards for the maximum number of accessory buildings and the maximum size of accessory buildings; and

**WHEREAS**, the proposed amendment would allow two accessory buildings on a residentially zoned property and would consider attached garages as part of the principal building.

**WHEREAS**, the maximum size for accessory buildings would remain consistent with the current standards.

**WHEREAS**, the Planning Commission opened a public hearing on the proposed resolution at their July 6, 2016 meeting and continued the public hearing to their August 3, 2016 meeting; and

**NOW THEREFORE, BE IT RESOLVED** by the Planning Commission of the City of South St. Paul, Minnesota, as follows:

1. Approval of An Amendment to Sections 118-121 of the City Code, regarding the Single Family Residence District and 118-208 of the City Code regarding Accessory Buildings.

Adopted this 7<sup>th</sup> day of September, 2016.

\_\_\_\_\_  
Chair

ATTEST:

\_\_\_\_\_  
City Planner

Sec. 118-121. - R-1, single-family district.

(a) *Permitted uses.* Within the R-1 single-family district, no building, structure or land shall be used except for one or more of the following uses, unless otherwise provided in this chapter:

(1) *Principal use.*

- a. One single-family dwelling (not to include mobile homes).
- b. Churches, chapels, temples, and synagogues, including Sunday schools, and parish houses meeting the requirements of this district.
- c. Public parks and playgrounds.

(2) *Accessory uses.*

- a. One garage ~~(attached or unattached)~~ used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted, as regulated herein by the performance standards section 118-208.
- b. One accessory building or structure, in addition to the garage, the use of which is incidental to and located on the same lot as the dwelling, as regulated herein by the performance standards section 118-208.
- c. Swimming pools and tennis courts, as regulated herein by the performance standards section.
- d. Keeping of domestic pets.
- e. Fences, signs, and recreation equipment, as regulated herein.
- f. Residential business office.
- g. A PWS antenna located entirely inside a building or structure.
- h. Keeping of chickens as per the backyard chicken regulations found in section 15-9.

(b) *Uses by conditional use permit.* Within the R-1 district, the following uses shall be by conditional use permit only:

- (1) Accessory buildings that exceed the height or size requirements in city code section 118-208.
- (2) Hospitals or sanatoriums, philanthropic and charitable institutions, except correctional institutions and animal hospitals. Any building permitted to be used shall have an appearance that shall be in appropriate harmony with the residential character of the area.
- (3) Essential services (public utility and municipal services).
- (4) Elementary, junior, and senior high schools and incidental and accessory uses, when situated on the same site or unit of property.
- (5) Cemeteries.
- (6) Home occupations.
- (7) A PWS antenna mounted on the outside of an institutional building or structure of any height, as defined in section 118-207, and any governmental building or structure.

(c) *Building height, width, and area requirement.* Within the R-1 district, residential dwellings must comply with the following:

- (1) Shall not exceed a height of three stories, or 28 feet above grade as defined in the Minnesota State Building Code;
- (2) Shall be a minimum of 24 feet wide; and
- (3) Shall have a minimum area footprint of 800 square feet.

(d) *Lot requirements.* Within the R-1 district, the following requirements shall apply:

- (1) *Lot area, width, and depth.* Each dwelling, together with its accessory buildings, shall be located on a lot having an area not less than 9,000 square feet, width of not less than 75 feet, and depth of not less than 120 feet, except that a dwelling may be erected on a lot platted prior to May 1, 1967, having less than the foregoing area and width but having no less than 4,500 square feet of lot area.
- (2) *Ingress and egress.* All lots shall front on a public street or have adequate ingress and egress to a public street.
- (3) *Percent of land use.* All dwellings and accessory buildings on any lot shall not cover more than 30 percent of the area of the lot (see article VII of this chapter for additional requirements), except that all dwellings and accessory buildings on a lot containing 5,000 square feet or less shall not cover more than 35 percent of the lot.
- (4) *Front yard.* No building shall be erected, reconstructed, altered or moved nearer to the front lot line than the average setback observed by residential buildings on the same side of the street and fronting thereon within the same block. Further, no part of the structure shall be closer than 25 feet to the street line on which it faces, except when the average setback is less than 25 feet.
- (5) *Side yard.* There shall be a side yard of not less than five feet along each side of each building located on an interior lot having a frontage of 60 feet or less; such side yard on a lot having a frontage of more than 60 feet shall be not less than nine feet; provided, however, that a garage having no basement below it may have a side yard of not less than five feet if located in the front two-thirds of the lot, and no less than three feet if located entirely within the rear one-third of the lot. There shall be a side yard of not less than nine feet on the street side of any structure constructed on a corner lot. No building shall be placed within ten feet of any dwelling unit on an adjacent lot.
- (6) *Rear yard.* Each lot shall have a rear yard of not less than 25 feet in depth, except that a garage may be constructed within the 25-foot rear yard. A garage shall have a setback from the rear property line of not less than three feet unless the entrance faces an alley or street, then the garage shall be set back no less than eight feet from the rear property line.
- (7) *Conditional use permit requirements.* All uses by conditional use permit shall provide such setback in front, side, and rear yards as the city council shall determine, taking into account the proposed use, the character of the surrounding area, the density of the area, and other relevant factors.
- (e) *Off-street loading and unloading berths.* In the R-1 district, off-street loading and unloading shall be governed by the provisions of article VII of this chapter.
- (f) *Prohibited uses.* The following uses are prohibited because they are not compatible with the purposes of the R-1 single-family district: PWS towers and antennas, except as permitted under subsections (a)(2)g and (b)(7) of this section.

(Code 1992, § 1500.09; Ord. No. 1267, § 1, 2-19-2013; Ord. No. 1297, § 1, 4-20-2015)

Sec. 118-208. - Accessory buildings and structures.

The requirements and regulations specified in this chapter shall be subject to the following:

- (a) *Purpose.* The purpose of this section is to regulate the number size, location and appearance of all buildings accessory to and detached from principal buildings on lots within the city. These regulations shall apply to all detached structures, including but not limited to garages, carports, storage buildings, gazebos, screen houses, play houses, and similar structures.
- (b) *Number.*
  - (1) Single-family residential zoning districts:
    - a. One ~~garage (attached or unattached)~~<sup>detached</sup> used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted.
    - b. One accessory building or structure in addition to the garage, the use of which is incidental to and located on the same lot as the dwelling.
  - (2) All other districts:
    - a. One per principal building.
- (c) *Location.*
  - (1) All zoning districts:
    - a. No accessory building shall be located nearer the front lot line than the principal building on that lot.
    - b. The required setbacks shall apply to all types of corner lots and accessory buildings shall adhere to any front or side setbacks as may be applicable to any principal building on such lots from both streets.
    - c. The minimum distance between the principal building and a detached accessory building shall be determined by the Minnesota State Building Code.
    - d. No building shall be placed within ten feet of any dwelling unit on an adjacent lot.
    - e. No accessory buildings shall be located within utility and drainage easements. It is the owner's responsibility to verify the location of the property lines.
  - (2) Single-family residential zoning districts:
    - a. Side yard setbacks:
      - i. Accessory buildings in a residential district must be located at least five feet from the side lot line in the front two-thirds of the lot and three feet in the rear one-third of the lot. Accessory buildings and projections (soffits or overhang) located less than five feet from the property line are subject to additional fire separation requirements from the Minnesota State Building Code.
      - ii. An accessory buildings must be located at least nine feet from the street side property line on a corner lot.
    - b. Rear yard setback:
      - i. A garage shall have a setback from the rear property line of not less than three feet unless the entrance faces an alley or street, then the garage shall be set back no less than eight feet from the rear property line. Buildings and projections (soffits or overhang) located less than five feet from a common property line are subject to additional fire separation requirements from the Minnesota State Building Code.

- ii. All other accessory buildings shall be set back at least three feet from the rear property line. Buildings and projections (soffits or overhang) located less than five feet from a common property line are subject to additional fire separation requirements from the Minnesota State Building Code.
    - c. Setbacks for through lots:
      - i. The minimum setback from the rear street of a through lot shall be 30 feet.
  - (3) All other districts:
    - a. Accessory buildings in the business and industrial districts may be located to the rear of the principal building, subject to rear setbacks, the Minnesota State Building Code and any fire separation requirements.
    - b. No accessory building in the industrial district shall be located less than ten feet from a rear or side lot line unless otherwise provided for herein.
- (d) *Maximum height.*
- (1) Single-family residential zoning districts:
    - a. Garages or any accessory structure intended to or capable of storing vehicles:
      - i. No detached garage shall exceed one story. As used in this subsection, the word "story" means the portion of the garage between the surface of the floor and the roof above.
      - ii. In the case of a hip roof or gable roof, no part of the garage shall exceed a height of 16 feet measured from the garage floor to the highest point of the roof; in all other cases the highest point of the garage shall not exceed a height of 12 feet above the surface of the garage floor.
      - iii. The exterior side walls of a garage shall not exceed ten feet in height.
    - b. All other accessory buildings:
      - i. The height of a detached building shall not exceed 12 feet. If attached, the structure shall not exceed the height of the principal building.
  - (2) All other districts:
    - a. No accessory building in a business or industrial district shall exceed the height of the principal building except by conditional use permit.
- (e) *Maximum size.*
- (1) Single-family residential zoning districts:
    - a. Garages or any accessory structure intended to or capable of storing vehicles:
      - i. The maximum size for a garage shall be 1,000 square feet.
    - b. All other accessory buildings:
      - i. Structures larger than 200 square feet shall require a conditional use permit.
    - c. The maximum allowable size for all accessory buildings combined (garages and other accessory buildings) is 1,200 square feet.
  - (2) All other districts:
    - a. Accessory buildings shall not exceed the footprint to the principal building to which they are accessory.
    - b. Accessory buildings larger than 200 square feet shall require a conditional use permit.
- (f) *Construction and finish.*
- (1) All zoning districts:
    - a. All accessory buildings shall require a surfaced floor, except greenhouses.
    - b.

Accessory buildings shall be anchored to a concrete slab, or otherwise securely fastened to the ground by other methods approved by the building inspection department.

- c. Exterior materials and finish must match or complement the exterior finish of the principal structure in material, color and texture. Exterior surfaces of all accessory buildings shall be maintained in new or like new condition, free from cracked and peeling paint, rusting and deteriorating materials.

(2) Single-family residential zoning districts:

- a. If constructed of metal, the accessory structure shall have prefinished enamel siding and roof.
- b. No galvanized siding or roofing shall be used.
- c. Galvanized steel-covered pole buildings are prohibited.
- d. Wood frame accessory buildings or structures shall conform to the Minnesota State Building Code and shall have one of the following types of siding: Masonite, shakes, redwood, exterior plywood panel, hardboard, decorative steel, decorative aluminum, vinyl, hardie-board, decorative fiberglass and/or rough-cut exterior siding, and the roofing material shall consist of asphalt shingles, standing-seam metal roofing, or when the pitch of the roof is less than 5/12, decorative rolled roofing will be permitted.

(3) All other districts:

- a. In business and industrial districts, all accessory structures, screen walls, and exposed areas of retaining walls shall be of a similar type, quality, and appearance as the principal structure.

(g) *Approvals.*

- (1) Except in single-family residential zoning districts, no accessory building shall be constructed, erected or installed without approval of final site plans and building plans by the city.
- (2) No accessory building occupying an area greater than 200 square feet shall be constructed erected or installed without a building permit issued by the city.
- (3) No accessory building or use shall be constructed or developed on a lot prior to construction of the principal building.
- (4) Two-story accessory buildings are not permitted.
- (5) No accessory building or structure shall be used for living purposes as a dwelling unit.

(h) *Other structures.*

- (1) Freestanding tents and canopies for the purpose of housing motor or recreational vehicles or storage are prohibited, except for municipal purposes in an industrial district.
- (2) Fish houses shall be stored as regulated herein by the exterior storage regulations in section 118-240.

(Ord. No. 1267, § 3, 2-19-2013)



- **ARTICLE IV. - DISTRICT REGULATIONS**
- **DIVISION 1. - GENERALLY**
- **Sec. 118-121. - R-1, single-family district.**

(a) *Permitted uses.* Within the R-1 single-family district, no building, structure or land shall be used except for one or more of the following uses, unless otherwise provided in this chapter:

(1) *Principal use.*

- a. One single-family dwelling (not to include mobile homes).
- b. Churches, chapels, temples, and synagogues, including Sunday schools, and parish houses meeting the requirements of this district.
- c. Public parks and playgrounds.

(2) *Accessory uses.*

- a. One attached garage (~~attached or unattached~~) used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted, as regulated herein by the performance standards section 118-208.
- b. One detached garage used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted, as regulated herein by the performance standards section 118-208.
- c. One accessory building or structure, in addition to the garage, the use of which is incidental to and located on the same lot as the dwelling, as regulated herein by the performance standards section 118-208.
- d. Swimming pools and tennis courts, as regulated herein by the performance standards section.
- e. Keeping of domestic pets.
- f. Fences, signs, and recreation equipment, as regulated herein.
- g. Residential business office.
- h. A PWS antenna located entirely inside a building or structure.

- i. Keeping of chickens as per the backyard chicken regulations found in section 15-9.

(b) *Uses by conditional use permit.* Within the R-1 district, the following uses shall be by conditional use permit only:

- (1) Accessory buildings that exceed the height or size requirements in city code section 118-208.
- (2) Hospitals or sanatoriums, philanthropic and charitable institutions, except correctional institutions and animal hospitals. Any building permitted to be used shall have an appearance that shall be in appropriate harmony with the residential character of the area.
- (3) Essential services (public utility and municipal services).
- (4) Elementary, junior, and senior high schools and incidental and accessory uses, when situated on the same site or unit of property.
- (5) Cemeteries.
- (6) Home occupations.
- (7) A PWS antenna mounted on the outside of an institutional building or structure of any height, as defined in section 118-207, and any governmental building or structure.

(c) *Building height, width, and area requirement.* Within the R-1 district, residential dwellings must comply with the following:

- (1) Shall not exceed a height of three stories, or 28 feet above grade as defined in the Minnesota State Building Code;
- (2) Shall be a minimum of 24 feet wide; and
- (3) Shall have a minimum area footprint of 800 square feet.

(d) *Lot requirements.* Within the R-1 district, the following requirements shall apply:

- (1) *Lot area, width, and depth.* Each dwelling, together with its accessory buildings, shall be located on a lot having an area not less than 9,000 square feet, width of not less than 75 feet, and depth of not less than 120 feet, except that a dwelling may be erected on a lot platted prior to May 1, 1967, having less than the foregoing area and width but having no less than 4,500 square feet of lot area.

- (2) *Ingress and egress.* All lots shall front on a public street or have adequate ingress and egress to a public street.
- (3) *Percent of land use.* All dwellings and accessory buildings on any lot shall not cover more than 30 percent of the area of the lot (see article VII of this chapter for additional requirements), except that all dwellings and accessory buildings on a lot containing 5,000 square feet or less shall not cover more than 35 percent of the lot.
- (4) *Front yard.* No building shall be erected, reconstructed, altered or moved nearer to the front lot line than the average setback observed by residential buildings on the same side of the street and fronting thereon within the same block. Further, no part of the structure shall be closer than 25 feet to the street line on which it faces, except when the average setback is less than 25 feet.
- (5) *Side yard.* There shall be a side yard of not less than five feet along each side of each building located on an interior lot having a frontage of 60 feet or less; such side yard on a lot having a frontage of more than 60 feet shall be not less than nine feet; provided, however, that a garage having no basement below it may have a side yard of not less than five feet if located in the front two-thirds of the lot, and no less than three feet if located entirely within the rear one-third of the lot. There shall be a side yard of not less than nine feet on the street side of any structure constructed on a corner lot. No building shall be placed within ten feet of any dwelling unit on an adjacent lot.
- (6) *Rear yard.* Each lot shall have a rear yard of not less than 25 feet in depth, except that a garage may be constructed within the 25-foot rear yard. A garage shall have a setback from the rear property line of not less than three feet unless the entrance faces an alley or street, then the garage shall be set back no less than eight feet from the rear property line.
- (7) *Conditional use permit requirements.* All uses by conditional use permit shall provide such setback in front, side, and rear yards as the city council shall determine, taking into account the proposed use, the character of the surrounding area, the density of the area, and other relevant factors.
- (e) *Off-street loading and unloading berths.* In the R-1 district, off-street loading and unloading shall be governed by the provisions of article VII of this chapter.

(f) *Prohibited uses.* The following uses are prohibited because they are not compatible with the purposes of the R-1 single-family district: PWS towers and antennas, except as permitted under subsections (a)(2)g and (b)(7) of this section.

(Code 1992, § 1500.09; Ord. No. 1267, § 1, 2-19-2013; Ord. No. 1297, § 1, 4-20-2015)

**Sec. 118-208. - Accessory buildings and structures.**

The requirements and regulations specified in this chapter shall be subject to the following:

(a) *Purpose.* The purpose of this section is to regulate the number size, location and appearance of all buildings accessory to and detached from principal buildings on lots within the city. These regulations shall apply to all detached structures, including but not limited to garages, carports, storage buildings, gazebos, screen houses, play houses, and similar structures.

(b) *Number.*

(1) Single-family residential zoning districts:

- a. One attached garage (~~attached or unattached~~) used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted.
- b. One unattached garage used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted.
- c. One accessory building or structure in addition to the garage(s), the use of which is incidental to and located on the same lot as the dwelling.

(2) All other districts:

- a. One per principal building.

(c) *Location.*

(1) All zoning districts:

- a. No accessory building shall be located nearer the front lot line than the principal building on that lot.
- b. The required setbacks shall apply to all types of corner lots and accessory buildings shall adhere to any front or side setbacks as may be applicable to any principal building on such lots from both streets.

c. The minimum distance between the principal building and a detached accessory building shall be determined by the Minnesota State Building Code.

d. No building shall be placed within ten feet of any dwelling unit on an adjacent lot.

e. No accessory buildings shall be located within utility and drainage easements. It is the owner's responsibility to verify the location of the property lines.

(2) Single-family residential zoning districts:

a. Side yard setbacks:

i. Accessory buildings in a residential district must be located at least five feet from the side lot line in the front two-thirds of the lot and three feet in the rear one-third of the lot. Accessory buildings and projections (soffits or overhang) located less than five feet from the property line are subject to additional fire separation requirements from the Minnesota State Building Code.

ii. An accessory buildings must be located at least nine feet from the street side property line on a corner lot.

b. Rear yard setback:

i. A detached garage shall have a setback from the rear property line of not less than three feet unless the entrance faces an alley or street, then the garage shall be set back no less than eight feet from the rear property line. Buildings and projections (soffits or overhang) located less than five feet from a common property line are subject to additional fire separation requirements from the Minnesota State Building Code.

ii. All other accessory buildings shall be set back at least three feet from the rear property line. Buildings and projections (soffits or overhang) located less than five feet from a common property line are subject to additional fire separation requirements from the Minnesota State Building Code.

c. Setbacks for through lots:

i. The minimum setback from the rear street of a through lot shall be 30 feet.

(3) All other districts:

a. Accessory buildings in the business and industrial districts may be located to the rear of the principal building, subject to rear setbacks, the Minnesota State Building Code and any fire separation requirements.

b. No accessory building in the industrial district shall be located less than ten feet from a rear or side lot line unless otherwise provided for herein.

(d) *Maximum height.*

(1) Single-family residential zoning districts:

a. Garages or any accessory structure intended to or capable of storing vehicles:

i. No detached garage shall exceed one story. As used in this subsection, the word "story" means the portion of the garage between the surface of the floor and the roof above.

ii. In the case of a hip roof or gable roof, no part of the garage shall exceed a height of 16 feet measured from the garage floor to the highest point of the roof; in all other cases the highest point of the garage shall not exceed a height of 12 feet above the surface of the garage floor.

iii. The exterior side walls of a garage shall not exceed 10 ~~ten~~ feet in height.

b. All other accessory buildings:

i. The height of a detached building shall not exceed 12 feet. If attached, the structure shall not exceed the height of the principal building.

(2) All other districts:

a. No accessory building in a business or industrial district shall exceed the height of the principal building except by conditional use permit.

(e) *Maximum size.*

(1) Single-family residential zoning districts:

a. Garages or any accessory structure intended to or capable of storing vehicles:

i. The maximum size for a garage shall be 1,000 square feet.

b. All other accessory buildings:

i. Structures larger than 200 square feet shall require a conditional use permit.

c. The maximum allowable size for all accessory buildings combined (garages and other accessory buildings) is 1,200 square feet. In the case of a property with a detached garage, an attached garage, and another accessory building the total size of all of these spaces shall not exceed 1,200 square feet.

(2) All other districts:

a. Accessory buildings shall not exceed the footprint to the principal building to which they are accessory.

b. Accessory buildings larger than 200 square feet shall require a conditional use permit.

(f) *Construction and finish.*

(1) All zoning districts:

a. All accessory buildings shall require a surfaced floor, except greenhouses.

b. Accessory buildings shall be anchored to a concrete slab, or otherwise securely fastened to the ground by other methods approved by the building inspection department.

c. Exterior materials and finish must match or complement the exterior finish of the principal structure in material, color and texture. Exterior surfaces of all accessory buildings shall be maintained in new or like new condition, free from cracked and peeling paint, rusting and deteriorating materials.

(2) Single-family residential zoning districts:

- a. If constructed of metal, the accessory structure shall have prefinished enamel siding and roof.
- b. No galvanized siding or roofing shall be used.
- c. Galvanized steel-covered pole buildings are prohibited.
- d. Wood frame accessory buildings or structures shall conform to the Minnesota State Building Code and shall have one of the following types of siding: Masonite, shakes, redwood, exterior plywood panel, hardboard, decorative steel, decorative aluminum, vinyl, hardie-board, decorative fiberglass and/or rough-cut exterior siding, and the roofing material shall consist of asphalt shingles, standing-seam metal roofing, or when the pitch of the roof is less than 5/12, decorative rolled roofing will be permitted.

(3) All other districts:

- a. In business and industrial districts, all accessory structures, screen walls, and exposed areas of retaining walls shall be of a similar type, quality, and appearance as the principal structure.

(g) *Approvals.*

- (1) Except in single-family residential zoning districts, no accessory building shall be constructed, erected or installed without approval of final site plans and building plans by the city.
- (2) No accessory building occupying an area greater than 200 square feet shall be constructed erected or installed without a building permit issued by the city.
- (3) No accessory building or use shall be constructed or developed on a lot prior to construction of the principal building.
- (4) Two-story accessory buildings are not permitted.
- (5) No accessory building or structure shall be used for living purposes as a dwelling unit.

(h) *Other structures.*

(1) Freestanding tents and canopies for the purpose of housing motor or recreational vehicles or storage are prohibited, except for municipal purposes in an industrial district.

(2) Fish houses shall be stored as regulated herein by the exterior storage regulations in [section 118-240](#).

(Ord. No. 1267, § 3, 2-19-2013)