



South St. Paul

MAYOR/COUNCIL WORKSESSION
SSP City Hall
125 3rd Avenue North

Monday, June 27, 2016
7:00 p.m.

AGENDA:

1. Environmental Committee Ideas – Caleb Werth- NO Attachment
2. Joint Recycling Coordinator Discussion
3. Discuss Comp Plan and Zoning modifications for NRDD district (NE corner of the city) to accommodate land lease with Union Pacific Railroad.
4. Discuss potential code changes regarding garages/accessory structures – Follow-up to discussion of 357 19th Avenue S. case.
5. Discuss – Construction of new building and rifle range at Gun Club
6. Grant application strategies – North Concord improvements, Wentworth sidewalk, and Southview Blvd.
7. 2017 Budget Discussion – NO Attachment



WORKSESSION AGENDA REPORT

DATE: June 27, 2016

DEPARTMENT: Licensing/Code Enforcement Division

ADMINISTRATOR: JPK

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AGENDA ITEM: Recycling Coordinator Program JPA

ACTION TO BE CONSIDERED:

Staff Recommendation: Staff recommends Council discussion and direction regarding the development and participation in a regional recycling program, starting in 2017.

OVERVIEW:

Communities recognize the critical need to explore innovative ways to improve our economic viability and service delivery efficiency. Since the operational changes to the Community Development Department and reassigned roles of Christy Wilcox, the recycling coordinator role is becoming a concern. The concerns relate to meeting the requirements of the funds received from the County with organic recycling being a priority issue for 2017. Organic recycling will include providing a lot of education to the community which will be very time consuming. The City of West St. Paul is also dealing with the role of the Recycling Coordinator with the recent resignation of their Recycling Coordinator.

Recycling responsibilities has historically consumed about 15% of a full time position. Because of the time constraints on other responsibilities of the Licensing/Code Enforcement Division Manager, as well as the redundant nature of recycling activities done by each community, City staff began to explore the possibility of a regional and more coordinated approach to accomplish the city's recycling responsibilities.

Over the past few months, the Cities of South St. Paul, West St. Paul, Inver Grove Heights and Mendota Heights have been meeting and discussing a joint recycling program and are all supportive of this idea. There is a similar successful model between Burnsville and Apple Valley. The four aforementioned cities have met monthly to outline a program, review a job description and a possible Joint Powers Agreement (JPA) agreement. The attached JPA is a draft that the City Attorney has prepared.

If the City decides to move forward with a Joint Recycling Coordinator, this staff person would be a West St. Paul employee and equally allocate time to all four Cities with West St. Paul being the "home base". The staff is anticipating the Recycling Coordinator would work 32 hours per week; one full day per week in each City.

A coordinated regional approach would not only allow for improved recycling programming, it would free up time in each community for those employees who are doing recycling as a secondary component of their job. The Licensing/Code Enforcement Manager will continue to manage the compost site and help in the annual clean-up day event.

SOURCE OF FUNDS:

Recycling activities are primarily funded through a \$24,000 County SCORE grant. These proceeds go toward funding salary and program costs. Under a regional approach, the communities would work together to utilize the collective SCORE grants to fund the joint recycling position and programming.

April 2002

**BURNSVILLE/EAGAN
POSITION DESCRIPTION**

POSITION CLASSIFICATION TITLE: Environmental Specialist
DEPARTMENT: Natural Resources
ACCOUNTABLE TO: Director of Natural Resources

JOB SUMMARY

Responsible for the coordination of the Eagan and Burnsville recycling programs and the development of policies and programs related to solid waste management. Acts as a liaison externally and internally in regard to solid waste management, waste stream reduction and recycling

ESSENTIAL JOB FUNCTIONS

1. Coordinate recycling programs to ensure county and state recycling goals are met. Encourage and monitor residential and business recycling programs. Provide motivation for recycling through program design. Survey residents and businesses to evaluate and adjust programs.
2. Develop, coordinate, and implement special promotions and public education programs encouraging waste stream reduction, diversion, and recycling. Coordinate special waste collections, Earth Day celebrations, and recycling presentations.
3. Act as a liaison for the City with other government entities, with interested community groups, agencies, and committees and the public regarding solid waste management planning, policies, and issues.
4. Prepare and administer a budget. Develop a budget each year that balances expenditures between Eagan and Burnsville.
5. Submit an annual grant to Dakota County to obtain funding. Research, implement, and evaluate other grant projects.
6. Work with the public, including neighborhood groups and other community organizations to achieve objectives for the collection of municipal solid waste.
7. Review City ordinances for compliance with County and State solid waste laws and license waste haulers by reviewing applications for compliance with City ordinance.

8. Supervise the Environmental Technician. Monitor activities, assist with development of goals and a workplan.
9. Monitor and evaluate Brownfield sites. Provide technical support for city staff. Work with other agencies to ensure appropriate closure of contaminated sites.
10. Respond to citizen complaints and questions.
11. Performs other duties and assumes other responsibilities as apparent or assigned

KNOWLEDGE, SKILLS AND ABILITIES

Ability to perform research, assemble information and prepare reports and educational material.

Knowledge of solid waste management and recycling practices.

Ability to prepare and administer a budget.

Ability to communicate effectively orally and in writing.

Ability to develop and maintain effective working relationships with other City personnel, other agencies, volunteer groups, and residents.

Ability to work independently, determine priorities, delegate and make appropriate decisions.

Knowledge of computer use and standard office equipment.

Ability to supervise the work of others.

QUALIFICATIONS

Completion of a Bachelor's Degree in public administration, environmental studies, physical sciences or a related field with and additional one to three years of administrative experience related to waste management

Must have a valid Minnesota driver's license

**AGREEMENT TO PROVIDE
RECYCLING COORDINATOR SERVICES**

THIS AGREEMENT is made this ____ day of _____, 2016, by and among the Cities of **WEST ST. PAUL, SOUTH ST. PAUL, INVER GROVE HEIGHTS** and **MENDOTA HEIGHTS** (hereinafter individually referred to as a “City” and collectively referred to as the “Cities”).

1. **AUTHORITY.** This Agreement is entered into pursuant to Minnesota Statute § 471.59.

2. **PURPOSE.** The purpose of this Agreement is to provide recycling coordination services for the Cities.

3. **RECYCLING COORDINATOR SERVICE.** A Recycling Coordinator will be hired as an employee of the City of West St. Paul, and shall perform the basic services of the recycling program (the “Program”) for itself and for the Cities of Inver Grove Heights, South St. Paul and Mendota Heights. The basic Program services include but are not limited to:

- Distribute recycling/disposal information to each household.
- Actively participate and contribute to monthly Local Solid Waste Staff Meetings with Dakota County.
- Support and promote Dakota County’s integrated solid waste management program.
- Make presentations (schools, community groups, local organizations).
- Work with Multi-family buildings to enhance recycling efforts.
- Work with City staff to reduce waste and increase environmentally preferable purchases.
- Ensure that recycling programs are established for City facilities.
- Apply for the annual grants from Dakota County commonly known as the Community Landfill Abatement Grants on behalf of each City.
- Produce the annual reports to Dakota County as required pursuant to the Community Landfill Abatement Grants.
- Comply with all requirements of the Dakota County Community Landfill Abatement Grants to maintain funding.

- Answer phone/e-mail inquiries regarding recycling/disposal.
- Assist Cities in promotion and administration of recycling events.

If any of the Cities of Inver Grove Heights, South St. Paul or Mendota Heights desire additional services outside of the scope of the basic services of the Program, that City may enter into a separate agreement with the City of West St. Paul concerning those services.

4. FINANCE.

A. The Cities of Inver Grove Heights, South St. Paul and Mendota Heights shall pay the City of West St. Paul to reimburse for the costs of wages, benefits and other costs necessary to employ the Recycling Coordinator and operate the Program. The Cities shall initially share the cost of this position equally in accordance with Attachment A. By _____ of each year, the Cities shall meet to review the results of the Program and shall establish a budget for the following year. The City of West St. Paul shall invoice each other City quarterly. Each City shall pay the invoice within thirty (30) days. The City of West St. Paul is authorized to pay claims submitted by the Recycling Coordinator.

B. The City of West St. Paul shall act as fiscal agent and shall maintain a separate fund for the purpose of operating the Program.

C. Any grants received (other than the Community Landfill Abatement Grants) will be apportioned equally among the Cities and shall reduce each City's contribution to the costs of the Program.

5. CONTRIBUTIONS OF FACILITIES BY CITIES.

A. Each City shall determine which of its assets will be available to the Program, but at a minimum each City will provide a workspace with appropriate office supplies and equipment to allow the Recycling Coordinator to fulfill the requirements of the Program. It

is expected that the Recycling Coordinator will spent at least one day a week officing in each City, with the final schedule to be determined at a later date.

B. Each City shall maintain public liability insurance coverage on the assets made available for the Program.

C. Assets made available to the Program will be promptly returned to the City that provided them upon that City's withdrawal from the Agreement.

D. The Recycling Coordinator in charge of the Program will be supervised by the designated contact in West St. Paul, in coordination with the primary contacts in the individual Cities. Required safety, legal and related reporting shall be through the designated contact in the City of West St. Paul.

6. **PERSONNEL.** The City of West St. Paul shall establish standards and qualifications for its personnel. The Recycling Coordinator shall be deemed an employee of the City of West St. Paul, not of the other individual Cities. The Recycling Coordinator shall be subject to the personnel and other policies of the City of West St. Paul.

7. **INSURANCE AND INDEMNIFICATION.**

A. General Liability Insurance. Each individual City agrees to maintain in force comprehensive general liability insurance equal to or greater than the maximum liability for tort claims under Minn. Stat. § 466.04, as amended. If any City is notified that its insurance is cancelled, it will immediately notify the other Cities in writing. If any City is unable to obtain or keep in force at least the minimum coverage required by this paragraph, any City may withdraw from this Agreement after giving the other member Cities at least thirty (30) days written notice of its intent to withdraw.

B. Workers' Compensation Insurance. Each City shall be responsible for injuries to or death of its own employees. Each City shall maintain workers' compensation coverage or self-insurance coverage, covering its own employees while they are providing services pursuant to this Agreement. Each City waives the right to sue any other City for any workers' compensation benefits paid to its own employee or their dependents, even if the injuries were caused wholly or partially by the negligence of any other City or its officers, employees or agents.

C. Indemnification. Each City shall be liable for its own acts to the extent provided by law. The Cities agree to indemnify and hold harmless each other and each other's respective employees, trustees, directors, officers, subcontractors, agents or other members of its workforce, each of the foregoing referred to as "indemnified party," against all actual and direct losses suffered by the indemnified party and all liability to third parties arising from or in connection with any breach of this Agreement or from any negligence or wrongful acts or omissions by the indemnifying party or its employees, trustees, directors, officers, subcontractors, agents or other members of its workforce in connection with the indemnifying party's performance under this Agreement.

Accordingly, on demand, the indemnifying party agrees to reimburse the indemnified party for any and all actual and direct losses, liabilities, lost profits, fines, penalties, costs or expenses (including reasonable attorneys' fees) which may for any reason be imposed upon any indemnified party by reason of any suit, claim, action, proceeding or demand by any third party that results from the indemnifying party's breach of any provision of this Agreement or from any negligence or wrongful acts or omissions by the indemnifying party or its

employees, trustees, directors, officers, subcontractors, agents or other members of its workforce in connection with the indemnifying party's performance under this Agreement.

Under no circumstances, however, shall a City be required to pay on behalf of itself and other parties to this Agreement any amounts in excess of the limits of liability established in Minn. Stat. § 466.04. The limits of liability for some or all of the Cities shall not be added together to determine the maximum amount of liability for any one City.

8. DURATION.

A. Any City may withdraw from this Agreement on _____ of any year. Written notice of termination must be given to the other Cities at least ninety (90) days prior thereto.

B. In the event of written notification to withdraw, the remaining Cities shall meet to consider modifying the Agreement to continue without the withdrawing City or to terminate. In the event of termination, all surplus funds shall be distributed to the Cities in proportion to the amount contributed over the lifetime of the Agreement, in relation to all contributions made by the Cities. Property obtained under this Agreement shall be distributed to the Cities in the same manner. If the remaining Cities continue this Agreement, the withdrawing City shall be given a portion of the surplus funds and property in proportion to the amount contributed by the City over the lifetime of the Agreement, in relation to all contributions made by the Cities.

9. PRIOR AGREEMENTS SUPERSEDED. This Agreement supersedes and repeals all prior agreements among the Cities related to the Recycling Coordinator.

10. NO PRESUMPTION AGAINST DRAFTING CITY. The Cities acknowledge that: (a) this Agreement and its reduction to final written form are the result of extensive good-

faith negotiations among the Cities through themselves and/or their respective legal counsel; (b) said Cities and/or their legal counsel have carefully reviewed and examined this Agreement prior to execution; and (c) any statute, common law, or rule of construction which provides that ambiguities are to be resolved against the drafting City(ies) shall not be employed in the interpretation of this Agreement.

11. GOVERNING LAW AND VENUE. This Agreement shall be governed and construed in accordance with the laws of the State of Minnesota without regard to its conflict of laws provision. The Cities agree that any action arising out of this Agreement or with respect to the enforcement of this Agreement shall be venued in the Dakota County District Court, State of Minnesota.

12. COUNTERPARTS. This Agreement may be executed in any number of counterparts, each of which when so executed shall be deemed to be an original and the counterparts shall together constitute one and the same agreement.

13. EXECUTION OF ADDITIONAL DOCUMENTS. The Cities are to execute and deliver to the other party, as requested, any additional documents and/or instruments that may reasonably be determined as necessary to consummate this transaction.

Dated: _____

CITY OF WEST ST. PAUL

By: _____
David Meisinger
Its Mayor

By: _____
Chantal Doriott
Its Clerk

Dated: _____

CITY OF SOUTH ST. PAUL

By: _____

Beth Baumann
Its Mayor

By: _____

Christy Wilcox
Its Clerk

Dated: _____

CITY OF INVER GROVE HEIGHTS

By: _____

George Tourville
Its Mayor

By: _____

Michelle Tesser
Its Clerk

Dated: _____

CITY OF MENDOTA HEIGHTS

By: _____

Sandra Krebsbach
Its Mayor

By: _____

Lorri Smith
Its Clerk

ATTACHMENT A
JOINT FEES/EXPENSES

Annual - 2016

- A. Recycling Coordinator Salary
- B. Benefits (PERA/FICA)
- C. Membership – Recycling Association of MN
- D. Printing/Promotion –generic materials
- E. Mileage Reimbursement
- F. Smart Phone Monthly Fee Reimbursement \$___/mo.
- G. Miscellaneous

Total Projected Costs

Divided between three Cities



COUNCIL WORKSESSION REPORT
DATE: JUNE 27, 2016
DEPARTMENT: Community Development
ADMINISTRATOR: SPK

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AGENDA ITEM: Comp Plan Amendments/Zoning Changes for NRDD

PROPOSED ACTION:

Discuss and provide direction.

OVERVIEW:

The City and Union Pacific Railroad have been discussing a potential lease of a 7-acre parcel at the NE corner of the City. However, the City would need to amend the Comprehensive Plan and Zoning district regulations before the City could consider an Interim Use Permit to allow the use or most other development scenarios.

COMP PLAN AMENDMENT:

The property is currently guided Open Space and would need to be amended to Industrial to accommodate development. The Met Council and surrounding cities and school districts would have to be notified and have a required comment period.

ISSUES FOR DISCUSSION:

The subject parcel is guided Open Space and was anticipated to remain greenspace. At the time the Comp Plan was updated planned development in the area was limited to the future development of Kaposia Landing (then Port Crosby) and a future off-street trail was anticipated for the area. The subject property has already been planned to accommodate an extension of the MRT trail into Saint Paul. One of the issues for long term land use planning is the potential conflict of use on the subject property with that trail connection if the property is accessed from the railroad, crossing the trail. Uses to the south and west include a strip of Railroad right of way with park just south of that strip of RR. Uses to the north in St. Paul are Industrial and the property to the east is currently an Industrial use (Alter Recycling).

ZONING:

The subject property is currently zoned NRDD and has a Public Land Overlay. The NRDD zone does not allow exterior storage. In order to consider an IUP for exterior storage the City would need to rezone the property or amend the zoning district to allow exterior storage in the NRDD zone.

OTHER ISSUES TO BE CONSIDERED:

1) Conflict of uses: Consistency of proposed/grant funded trail with exterior storage use. The proposed UPRR use involves storing rail cars in addition to products and would need to have RR tracks crossing the trail. This is proposed to be an at grade crossing which presents an

issue of usability (makes trail unusable for rollerbladers and less inviting for stroller users and those with disabilities) and safety. Would UPRR be able to bridge the trail over that access? The trail would continue south and bridge over another UPRR line to connect to Kaposia Landing.

2) Materials stored? Types of products? Height of storage? Pallets/stacked? Screening?

STAFF RECOMMENDATIONS:

The site does not have street access which poses a problem for development and for providing emergency access to SMFD, SSPPD, etc. However, if an access easement was secured the site could provide options for redevelopment to accommodate possible relocation of existing industrial uses in SSP without interfering with the extension of the MRT trail. Staff would recommend securing an access easement (St. Paul Port Authority, etc.). The proposed trail has more potential benefit to SSP residents and tourists and potential impacts to the trail need to be scrutinized. Staff also believes that the parcel offers more potential for development and the space could be critical in making redevelopment work for other sites in the community.

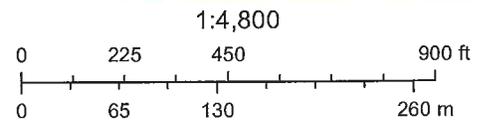
NEXT STEPS:

Staff has noticed this for a Public Hearing for the July meeting of the Planning Commission in case the City Council wants this to proceed for July/August meetings.

NRDD - City Owned Parcel



June 17, 2016





COUNCIL WORKSESSION REPORT
DATE: JUNE 27, 2016
DEPARTMENT: Community Development
ADMINISTRATOR: SPK

4

AGENDA ITEM: Code Changes for Garages and Accessory Structures

PROPOSED ACTION:

Discuss and provide staff with direction.

OVERVIEW:

At their June 6th meeting the City Council heard a request for a variance that involved 2 accessory structures and an attached garage. The Council asked staff to explore ways to allow similar proposals.

ZONING FOR GARAGES:

Language for garages/sheds in the R-1 section (118-121) and Accessory Buildings section (118-208) only allows one garage whether attached or detached. However, the setbacks etc., for attached garages are already covered as the setbacks for houses. This places more of a burden on properties with attached garages - particularly @-stall tuck-under garages which are often too small for many vehicles driven today.

One way to correct this would be to eliminate references in those code sections to "one garage attached or detached" and just allow 2 accessory buildings. Keeping the existing size standards would still allow them a very generous 1,200 s.f. (1,000 sf garage, 200 sf shed) where most garages are typically 400-576 scenarios (20 x 20, 24 x 24). If a property owner wanted a second structure over 200 sf (second detached garage) they would still need a CUP like they currently do.

NEXT STEPS:

Staff has noticed a Public Hearing so that this could be heard by the Planning Commission at their July meeting and it could be heard by the City Council at their meetings in July and August.

Sec. 118-121. - R-1, single-family district.

(a) *Permitted uses.* Within the R-1 single-family district, no building, structure or land shall be used except for one or more of the following uses, unless otherwise provided in this chapter:

(1) *Principal use.*

- a. One single-family dwelling (not to include mobile homes).
- b. Churches, chapels, temples, and synagogues, including Sunday schools, and parish houses meeting the requirements of this district.
- c. Public parks and playgrounds.

(2) *Accessory uses.*

- a. One garage ~~(attached or unattached)~~ used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted, as regulated herein by the performance standards section 118-208.
- b. One accessory building or structure, in addition to the garage, the use of which is incidental to and located on the same lot as the dwelling, as regulated herein by the performance standards section 118-208.
- c. Swimming pools and tennis courts, as regulated herein by the performance standards section.
- d. Keeping of domestic pets.
- e. Fences, signs, and recreation equipment, as regulated herein.
- f. Residential business office.
- g. A PWS antenna located entirely inside a building or structure.
- h. Keeping of chickens as per the backyard chicken regulations found in section 15-9.

(b) *Uses by conditional use permit.* Within the R-1 district, the following uses shall be by conditional use permit only:

- (1) Accessory buildings that exceed the height or size requirements in city code section 118-208.
- (2) Hospitals or sanatoriums, philanthropic and charitable institutions, except correctional institutions and animal hospitals. Any building permitted to be used shall have an appearance that shall be in appropriate harmony with the residential character of the area.
- (3) Essential services (public utility and municipal services).
- (4) Elementary, junior, and senior high schools and incidental and accessory uses, when situated on the same site or unit of property.
- (5) Cemeteries.
- (6) Home occupations.
- (7) A PWS antenna mounted on the outside of an institutional building or structure of any height, as defined in section 118-207, and any governmental building or structure.

(c) *Building height, width, and area requirement.* Within the R-1 district, residential dwellings must comply with the following:

- (1) Shall not exceed a height of three stories, or 28 feet above grade as defined in the Minnesota State Building Code;
- (2) Shall be a minimum of 24 feet wide; and
- (3) Shall have a minimum area footprint of 800 square feet.

(d) *Lot requirements.* Within the R-1 district, the following requirements shall apply:

- (1) *Lot area, width, and depth.* Each dwelling, together with its accessory buildings, shall be located on a lot having an area not less than 9,000 square feet, width of not less than 75 feet, and depth of not less than 120 feet, except that a dwelling may be erected on a lot platted prior to May 1, 1967, having less than the foregoing area and width but having no less than 4,500 square feet of lot area.
- (2) *Ingress and egress.* All lots shall front on a public street or have adequate ingress and egress to a public street.
- (3) *Percent of land use.* All dwellings and accessory buildings on any lot shall not cover more than 30 percent of the area of the lot (see article VII of this chapter for additional requirements), except that all dwellings and accessory buildings on a lot containing 5,000 square feet or less shall not cover more than 35 percent of the lot.
- (4) *Front yard.* No building shall be erected, reconstructed, altered or moved nearer to the front lot line than the average setback observed by residential buildings on the same side of the street and fronting thereon within the same block. Further, no part of the structure shall be closer than 25 feet to the street line on which it faces, except when the average setback is less than 25 feet.
- (5) *Side yard.* There shall be a side yard of not less than five feet along each side of each building located on an interior lot having a frontage of 60 feet or less; such side yard on a lot having a frontage of more than 60 feet shall be not less than nine feet; provided, however, that a garage having no basement below it may have a side yard of not less than five feet if located in the front two-thirds of the lot, and no less than three feet if located entirely within the rear one-third of the lot. There shall be a side yard of not less than nine feet on the street side of any structure constructed on a corner lot. No building shall be placed within ten feet of any dwelling unit on an adjacent lot.
- (6) *Rear yard.* Each lot shall have a rear yard of not less than 25 feet in depth, except that a garage may be constructed within the 25-foot rear yard. A garage shall have a setback from the rear property line of not less than three feet unless the entrance faces an alley or street, then the garage shall be set back no less than eight feet from the rear property line.
- (7) *Conditional use permit requirements.* All uses by conditional use permit shall provide such setback in front, side, and rear yards as the city council shall determine, taking into account the proposed use, the character of the surrounding area, the density of the area, and other relevant factors.
- (e) *Off-street loading and unloading berths.* In the R-1 district, off-street loading and unloading shall be governed by the provisions of article VII of this chapter.
- (f) *Prohibited uses.* The following uses are prohibited because they are not compatible with the purposes of the R-1 single-family district: PWS towers and antennas, except as permitted under subsections (a)(2)g and (b)(7) of this section.

(Code 1992, § 1500.09; Ord. No. 1267, § 1, 2-19-2013; Ord. No. 1297, § 1, 4-20-2015)

Sec. 118-208. - Accessory buildings and structures.

The requirements and regulations specified in this chapter shall be subject to the following:

- (a) *Purpose.* The purpose of this section is to regulate the number size, location and appearance of all buildings accessory to and detached from principal buildings on lots within the city. These regulations shall apply to all detached structures, including but not limited to garages, carports, storage buildings, gazebos, screen houses, play houses, and similar structures.
- (b) *Number.*
 - (1) Single-family residential zoning districts:
 - a. One ~~garage (attached or unattached)~~^{detached} used as an accessory to the dwelling and located upon the same lot, intended or capable of providing for the storage of motor vehicles and in which no business, occupation or service for profit is in any way conducted.
 - b. One accessory building or structure in addition to the garage, the use of which is incidental to and located on the same lot as the dwelling.
 - (2) All other districts:
 - a. One per principal building.
- (c) *Location.*
 - (1) All zoning districts:
 - a. No accessory building shall be located nearer the front lot line than the principal building on that lot.
 - b. The required setbacks shall apply to all types of corner lots and accessory buildings shall adhere to any front or side setbacks as may be applicable to any principal building on such lots from both streets.
 - c. The minimum distance between the principal building and a detached accessory building shall be determined by the Minnesota State Building Code.
 - d. No building shall be placed within ten feet of any dwelling unit on an adjacent lot.
 - e. No accessory buildings shall be located within utility and drainage easements. It is the owner's responsibility to verify the location of the property lines.
 - (2) Single-family residential zoning districts:
 - a. Side yard setbacks:
 - i. Accessory buildings in a residential district must be located at least five feet from the side lot line in the front two-thirds of the lot and three feet in the rear one-third of the lot. Accessory buildings and projections (soffits or overhang) located less than five feet from the property line are subject to additional fire separation requirements from the Minnesota State Building Code.
 - ii. An accessory buildings must be located at least nine feet from the street side property line on a corner lot.
 - b. Rear yard setback:
 - i. A garage shall have a setback from the rear property line of not less than three feet unless the entrance faces an alley or street, then the garage shall be set back no less than eight feet from the rear property line. Buildings and projections (soffits or overhang) located less than five feet from a common property line are subject to additional fire separation requirements from the Minnesota State Building Code.

- ii. All other accessory buildings shall be set back at least three feet from the rear property line. Buildings and projections (soffits or overhang) located less than five feet from a common property line are subject to additional fire separation requirements from the Minnesota State Building Code.

c. Setbacks for through lots:

- i. The minimum setback from the rear street of a through lot shall be 30 feet.

(3) All other districts:

- a. Accessory buildings in the business and industrial districts may be located to the rear of the principal building, subject to rear setbacks, the Minnesota State Building Code and any fire separation requirements.
- b. No accessory building in the industrial district shall be located less than ten feet from a rear or side lot line unless otherwise provided for herein.

(d) *Maximum height.*

(1) Single-family residential zoning districts:

- a. Garages or any accessory structure intended to or capable of storing vehicles:
 - i. No detached garage shall exceed one story. As used in this subsection, the word "story" means the portion of the garage between the surface of the floor and the roof above.
 - ii. In the case of a hip roof or gable roof, no part of the garage shall exceed a height of 16 feet measured from the garage floor to the highest point of the roof; in all other cases the highest point of the garage shall not exceed a height of 12 feet above the surface of the garage floor.
 - iii. The exterior side walls of a garage shall not exceed ten feet in height.
- b. All other accessory buildings:
 - i. The height of a detached building shall not exceed 12 feet. If attached, the structure shall not exceed the height of the principal building.

(2) All other districts:

- a. No accessory building in a business or industrial district shall exceed the height of the principal building except by conditional use permit.

(e) *Maximum size.*

(1) Single-family residential zoning districts:

- a. Garages or any accessory structure intended to or capable of storing vehicles:
 - i. The maximum size for a garage shall be 1,000 square feet.
- b. All other accessory buildings:
 - i. Structures larger than 200 square feet shall require a conditional use permit.
- c. The maximum allowable size for all accessory buildings combined (garages and other accessory buildings) is 1,200 square feet.

(2) All other districts:

- a. Accessory buildings shall not exceed the footprint to the principal building to which they are accessory.
- b. Accessory buildings larger than 200 square feet shall require a conditional use permit.

(f) *Construction and finish.*

(1) All zoning districts:

- a. All accessory buildings shall require a surfaced floor, except greenhouses.
- b.

Accessory buildings shall be anchored to a concrete slab, or otherwise securely fastened to the ground by other methods approved by the building inspection department.

- c. Exterior materials and finish must match or complement the exterior finish of the principal structure in material, color and texture. Exterior surfaces of all accessory buildings shall be maintained in new or like new condition, free from cracked and peeling paint, rusting and deteriorating materials.

(2) Single-family residential zoning districts:

- a. If constructed of metal, the accessory structure shall have prefinished enamel siding and roof.
- b. No galvanized siding or roofing shall be used.
- c. Galvanized steel-covered pole buildings are prohibited.
- d. Wood frame accessory buildings or structures shall conform to the Minnesota State Building Code and shall have one of the following types of siding: Masonite, shakes, redwood, exterior plywood panel, hardboard, decorative steel, decorative aluminum, vinyl, hardie-board, decorative fiberglass and/or rough-cut exterior siding, and the roofing material shall consist of asphalt shingles, standing-seam metal roofing, or when the pitch of the roof is less than 5/12, decorative rolled roofing will be permitted.

(3) All other districts:

- a. In business and industrial districts, all accessory structures, screen walls, and exposed areas of retaining walls shall be of a similar type, quality, and appearance as the principal structure.

(g) *Approvals.*

- (1) Except in single-family residential zoning districts, no accessory building shall be constructed, erected or installed without approval of final site plans and building plans by the city.
- (2) No accessory building occupying an area greater than 200 square feet shall be constructed erected or installed without a building permit issued by the city.
- (3) No accessory building or use shall be constructed or developed on a lot prior to construction of the principal building.
- (4) Two-story accessory buildings are not permitted.
- (5) No accessory building or structure shall be used for living purposes as a dwelling unit.

(h) *Other structures.*

- (1) Freestanding tents and canopies for the purpose of housing motor or recreational vehicles or storage are prohibited, except for municipal purposes in an industrial district.
- (2) Fish houses shall be stored as regulated herein by the exterior storage regulations in section 118-240.

(Ord. No. 1267, § 3, 2-19-2013)



COUNCIL WORKSESSION REPORT
DATE: JUNE 27, 2016
DEPARTMENT: Community Development
ADMINISTRATOR: JK

AGENDA ITEM: Gun Club CUP/Site Plan Discussion

PROPOSED ACTION:

Review and discuss proposed plans.

BACKGROUND:

Previously the Council approved berming to surround a proposed rifle range on the southern portion of the Gun Club property. They indicated that the shooters would be firing through a culvert structure and between that and the berms that all bullets would be kept on site. At the meeting they indicated that a building may be used in the future in place of the culvert structure.

Earlier this year the City Planner and City Engineers toured the site with the representatives from the Gun Club and found that the firing area was a semi-trailer with concrete culverts set in front of holes cut in the side of the trailer for firing stations. The trailer was not something approved by the Council and would best be classified as a temporary building which are only allowed for 14 days at a time for 2x per year. In addition, the structure would not meet architectural standards. Staff also had some reservations about the placement of the culverts and was concerned whether they would perform properly with settling, etc. Staff also noticed another trailer had been installed as a permanent fixture as a shooting station at the shotgun range which also had not been approved by the Council.

The plans submitted by the SSP Gun Club show a wood framed structure with a Fiber-Cement exterior and would be enclosed on 3 sides (firing sides would have a knee wall and would be open above). The proposal also includes several baffles structures set at intervals to prevent bullets from being fired up in the air so they would not leave the site. The structure exceeds 200 sf so a CUP is required.

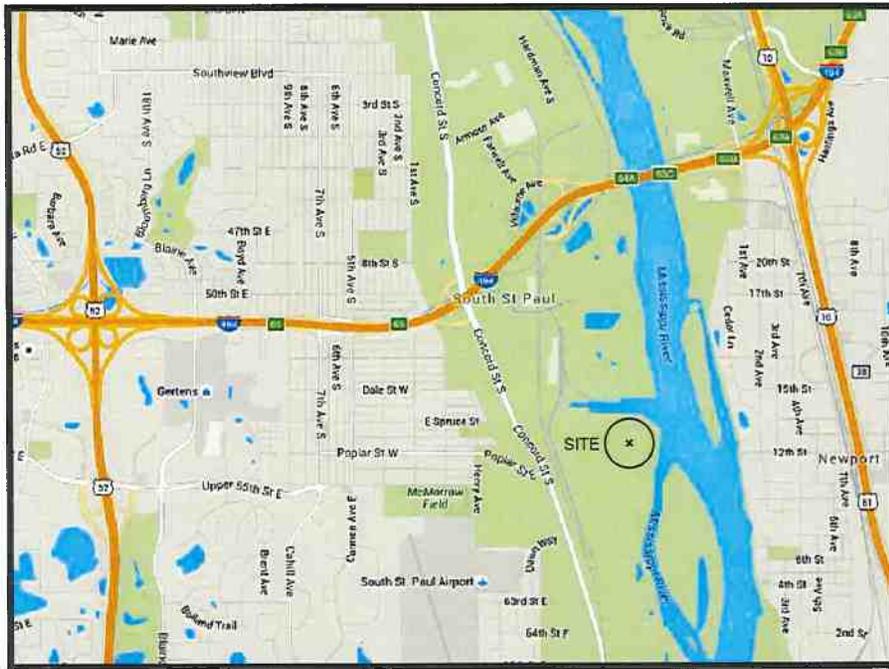
NEXT STEPS:

The SSP Gun Club CUP/Site Plan is on the Planning Commission agenda for July. A work session with representatives from the Gun Club could be scheduled between the Planning Commission meeting and City Council meeting.

SOUTH SAINT PAUL ROD & GUN CLUB

600 GUN CLUB ROAD SAINT PAUL, MINNESOTA RANGE IMPROVEMENTS

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07	FIRING LINE SHED FOUNDATION PLAN
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10	BERM SECTIONS
11	ECOLOGY BLOCK WALL AND BULLET TRAP SECTIONS AND DETAILS
12	CONCEPT RIFLE EYEBROW
13	CIVIL DETAILS



VICINITY MAP
NTS

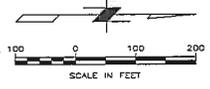
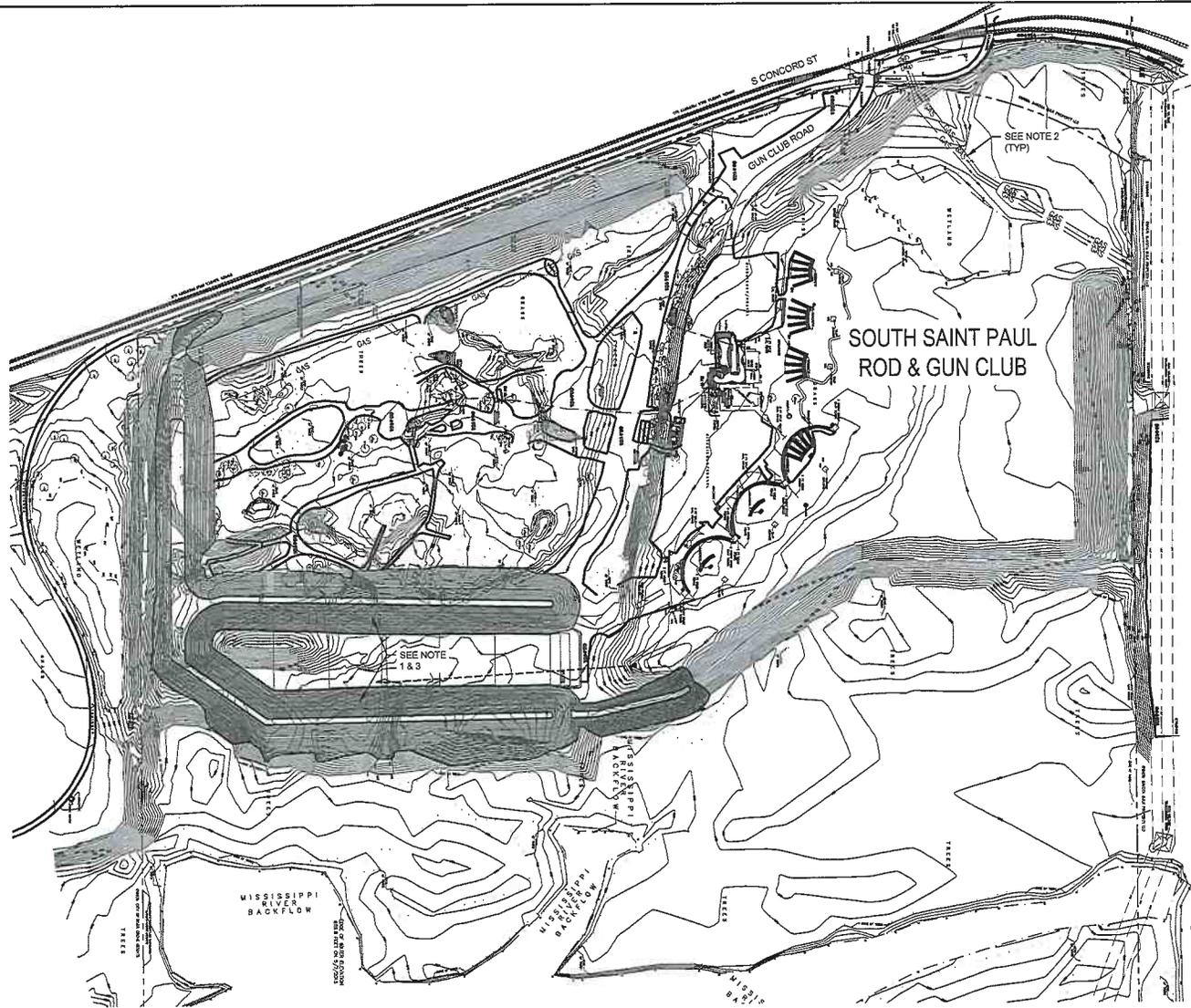


SITE PLAN
NTS

J:\0484412 South St. Paul ROD SAR\000_CAD\05-Sheets\01-Cover.dwg User:peterremus Plotted:Jun 02, 2016 - 7:00am Lost Save:Jun 02, 2016 - 7:00am

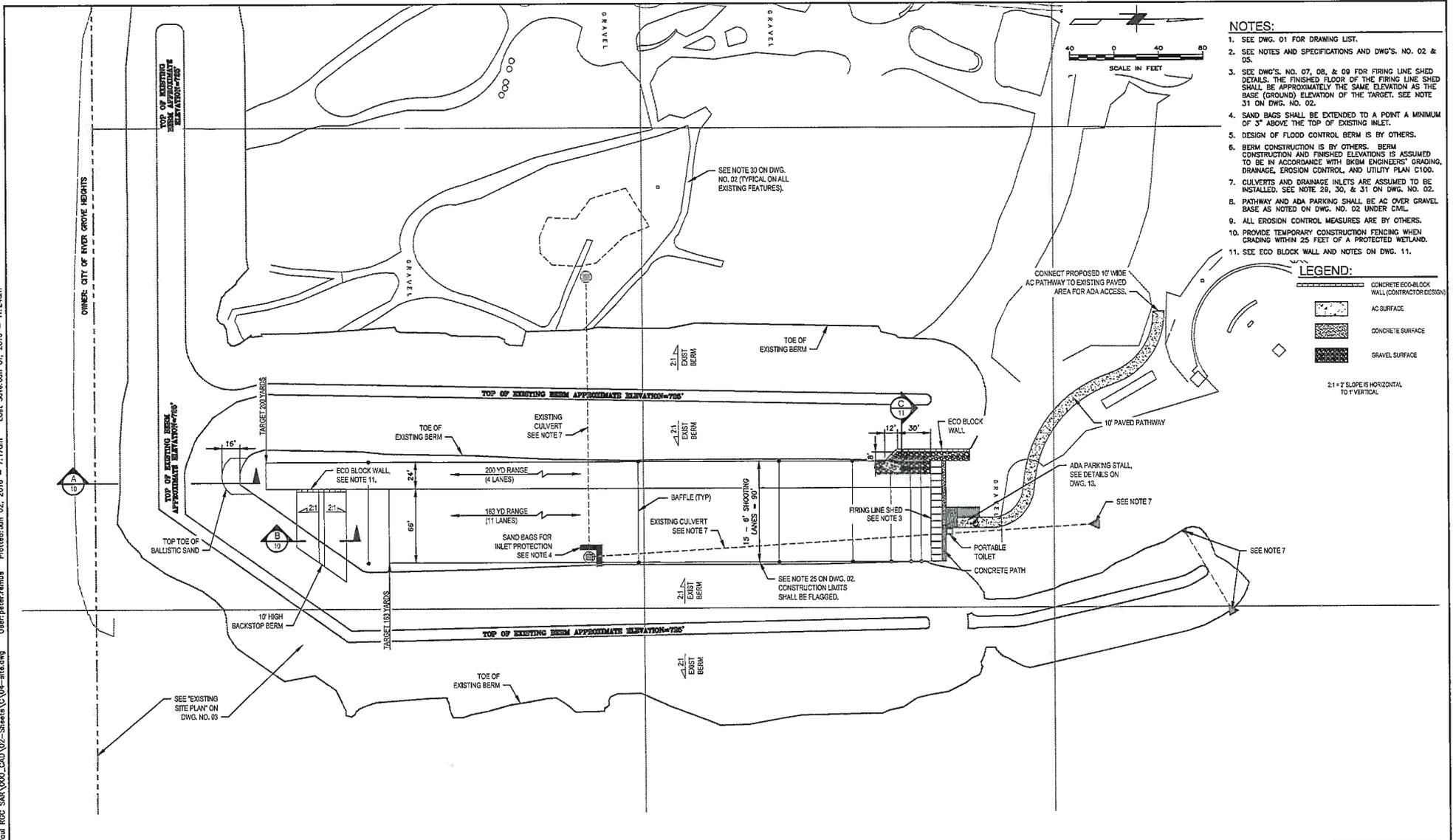
<p>PRELIMINARY NOT FOR CONSTRUCTION</p>		JOB No. 60484412	DESIGNED: PS	PROJ. ENGINEER: MLT		DAKOTA COUNTY, MN	SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT	DRAWING NUMBER: 01								
		SCALE: AS NOTED	DRAWN BY: JG	APPROVED BY: SK				CAD FILE NUMBER: 01-Cover								
<table border="1"> <thead> <tr> <th>No.</th> <th>DATE</th> <th>BY</th> <th>REVISION</th> </tr> </thead> <tbody> <tr> <td> </td> <td>06/02/16</td> <td>SK</td> <td>FOR FINAL REVIEW</td> </tr> </tbody> </table>		No.	DATE	BY	REVISION		06/02/16	SK	FOR FINAL REVIEW	CHECKED BY: MLT	DATE: JUNE 2016	111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com	600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	COVER SHEET	SHEET: 1 of 13	REV. B
No.	DATE	BY	REVISION													
	06/02/16	SK	FOR FINAL REVIEW													

J:\60484412 South St. Paul REC SAR\WOOD\CAD\02-Sheets\03-exist.dwg User: jeferrermus Plotted: Jun 01, 2016 -- 9:24am Lot Size: May 25, 2016 -- 3:59pm



- NOTES:**
1. SEE DRAWING No. 04 FOR PROPOSED IMPROVEMENTS.
 2. SEE DRAWING No. 02 & 05 FOR GENERAL, CIVIL AND STRUCTURAL NOTES.
 3. THE GRADING, DRAINAGE, EROSION CONTROL AND UTILITY PLAN, C100 COMPLETED FOR THE OWNER BY BKBM HAS BEEN MERGED INTO THE EXISTING TOPOGRAPHY AS IT IS ASSUMED THIS WORK HAS OR WILL BE COMPLETED PRIOR TO THE RANGE IMPROVEMENTS.

<p>PRELIMINARY NOT FOR CONSTRUCTION</p>				JOB No. 60484412		DESIGNED: PS		PROJ. ENGINEER: MLT			DAKOTA COUNTY, MN	SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT		DRAWING NUMBER: 03	
				SCALE: AS NOTED		DRAWN BY: JG		APPROVED BY: SK				111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com		600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	
B 06/02/16 SK FOR FINAL REVIEW		CHECKED BY: MLT		DATE: JUNE 2016								SHEET: REV. 3 of 13 B			
No. DATE BY		REVISION													



- NOTES:**
1. SEE DWG. 01 FOR DRAWING LIST.
 2. SEE NOTES AND SPECIFICATIONS AND DWG'S. NO. 02 & 05.
 3. SEE DWG'S. NO. 07, 08, & 09 FOR FIRING LINE SHED DETAILS. THE FINISHED FLOOR OF THE FIRING LINE SHED SHALL BE APPROXIMATELY THE SAME ELEVATION AS THE BASE (GROUND) ELEVATION OF THE TARGET. SEE NOTE 31 ON DWG. NO. 02.
 4. SAND BAGS SHALL BE EXTENDED TO A POINT A MINIMUM OF 3' ABOVE THE TOP OF EXISTING INLET.
 5. DESIGN OF FLOOD CONTROL BERM IS BY OTHERS.
 6. BERM CONSTRUCTION IS BY OTHERS. BERM CONSTRUCTION AND FINISHED ELEVATIONS IS ASSUMED TO BE IN ACCORDANCE WITH BGRM ENGINEERS' GRADING, DRAINAGE, EROSION CONTROL, AND UTILITY PLAN C100.
 7. CULVERTS AND DRAINAGE INLETS ARE ASSUMED TO BE INSTALLED. SEE NOTE 28, 30, & 31 ON DWG. NO. 02.
 8. PATHWAY AND ADA PARKING SHALL BE AC OVER GRAVEL BASE AS NOTED ON DWG. NO. 02 UNDER CIVIL.
 9. ALL EROSION CONTROL MEASURES ARE BY OTHERS.
 10. PROVIDE TEMPORARY CONSTRUCTION FENCING WHEN GRADING WITHIN 25 FEET OF A PROTECTED WETLAND.
 11. SEE ECO BLOCK WALL AND NOTES ON DWG. 11.

LEGEND:

- CONCRETE ECO-BLOCK WALL (CONTRACTOR DESIGN)
- AC SURFACE
- CONCRETE SURFACE
- GRAVEL SURFACE

21 = 7 SLOPE IS HORIZONTAL TO 1 VERTICAL.

<p>PRELIMINARY NOT FOR CONSTRUCTION</p>				JOB No. 60484412	DESIGNED: PS	PRD.J. ENGINEER: MLT		DAKOTA COUNTY, MN	SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT	DRAWING NUMBER: 04	
				SCALE: AS NOTED	DRAWN BY: JG	APPROVED BY: SK				CAD FILE NUMBER: 04-SITE	
C 06/02/16	SK FOR FINAL REVIEW			CHECKED BY: MLT	DATE: JUNE 2016	111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com		600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	SITE DEVELOPMENT PLAN	SHEET: 4 OF 13	REV. C

CIVIL NOTES

- CML**
1. ALL CIVIL MATERIALS, INSTALLATION AND TESTING SHALL CONFORM TO THE DAKOTA COUNTY STANDARD'S, WHERE A MATERIAL IS NOT ADDRESSED IN THE ABOVE COUNTY DOCUMENT THE CONTRACTOR SHALL CONFORM TO THE APPROPRIATE MNDOT STANDARDS AND SPECIFICATION.
 2. ALL CIVIL UTILITIES WORK SHALL CONFORM TO THE DAKOTA COUNTY, CITY, AND/OR OWNER OF THE UTILITY DESIGN AND CONSTRUCTION REQUIREMENTS. THE OWNER IS RESPONSIBLE TO WORK WITH THE CONTRACTOR AND UTILITY COMPANY WHEN UTILITIES ARE INSTALLED WITHIN THE NEW CONSTRUCTION.
 3. CONTRACTOR SHALL CONFORM TO ALL COUNTY, STATE AND FEDERAL LAWS IN FILLING OR REMOVING WETLANDS.
 4. 1 ADA PARKING SHALL HAVE BEEN INCLUDED IN THESE PLANS. SEE DRAWING NO. 04, SITE DEVELOPMENT PLAN FOR LOCATION.
 5. ENTRANCE ROAD AND SITE ROADWAYS TO BE PRIVATELY MAINTAINED. (BY OTHERS).

- MATERIALS**
1. BALLISTIC SAND TO BE CLEAN, CRUSHED, ANGULAR SAND, MIXED WITH LIMESTONE TO ACHIEVE A PH BETWEEN 6.5 AND 8.5, AND MEET THE FOLLOWING GRADATION:
- | SIZE | PERCENT PASSING |
|---------|-----------------|
| 3/8 | 100 |
| No. 4 | 85 - 100 |
| No. 8 | 80 - 100 |
| No. 16 | 50 - 85 |
| No. 30 | 5 - 10 |
| No. 50 | 5 - 30 |
| No. 200 | 0 - 10 |

2. SAND FOR BLANKET DRAIN SHALL CONFORM TO MNDOT STANDARD SPECIFICATIONS.
3. SEED MIXTURES SHALL CONFORM TO MNDOT ROADSIDE VEGETATION MANAGEMENT STANDARDS.
4. STRUCTURAL FILL TO MNDOT STANDARD SPECIFICATIONS.
5. COMPACT STRUCTURAL FILL TO 95% TO THE TESTS REQUIRED BY MNDOT; MAXIMUM LIFTS OF 8".
6. BORROW MATERIAL SHALL BE ON-SITE OR IMPORTED MATERIAL, NO GREATER THAN 3 INCHES IN DIAMETER, FREE OF UNSUITABLE MATERIALS OR OTHER CHARACTERISTICS DETRIMENTAL TO THE CONSTRUCTION OF FIRM, DENSE, AND SOUND EMBANKMENTS.
7. COMPACT BORROW MATERIAL AS PER NOTE 5 ABOVE.
8. DRAIN ROCK SHALL CONFORM TO MNDOT STANDARD SPECIFICATIONS.
9. TOPSOIL SHALL CONFORM TO MNDOT STANDARD SPECIFICATIONS, WITH THE EXCEPTION THAT THE PH SHALL BE BETWEEN 6.5 AND 8.5.
10. CORE MATERIAL SHALL BE ON-SITE OR IMPORTED MATERIAL, NO GREATER THAN 8 INCHES IN DIAMETER, FREE OF UNSUITABLE MATERIALS OR OTHER CHARACTERISTICS DETRIMENTAL TO THE CONSTRUCTION OF FIRM, DENSE, AND SOUND EMBANKMENTS.
11. COMPACT CORE MATERIAL SUFFICIENT TO MAINTAIN 2:1 SLOPES AND AS APPROVED BY A QUALIFIED INSPECTOR.
12. ALL SUBGRADES AND BACKFILLS TO BE APPROVED BY A QUALIFIED INSPECTOR.
13. GEOTEXTILE TO CONFORM TO MNDOT STANDARD SPECIFICATION.
14. ALL AREAS TO RECEIVE FILL SHALL BE STRIPPED OF UNSUITABLE MATERIAL AND PROPER ROLLED UNDER THE OBSERVATION OF A QUALIFIED INSPECTOR AND APPROVED PRIOR TO PLACING FILL MATERIALS.
15. EXCAVATION OF ON-SITE MATERIALS SHALL CONFORM TO MNDOT STANDARD SPECIFICATIONS, OR AS APPROVED BY THE OWNER.
16. BASE COURSE SHALL CONFORM MNDOT STANDARDS (SEE CIVIL NOTES ON SHEET 2).
17. COURSE ROCK SHALL CONFORM TO MNDOT STANDARDS (SEE CIVIL NOTES ON SHEET 2).
18. RIPRAP SHALL BE D50 = 2 1/2 INCHES WITH A MAX. DIAMETER OF 3 1/2 INCHES AS PER MNDOT STANDARDS SPECIFICATIONS.
19. CONCRETE SIDEWALKS SHALL CONFORM TO MNDOT STANDARD SPECIFICATIONS.
20. AGGREGATE SHALL CONFORM TO MNDOT STANDARD SPECIFICATIONS.
21. AREAS WITH NATIVE LOAM SOILS REQUIRING AGGREGATE ADDITION TO BE BLENDED AT A RATE OF 2.5 TONS/ACRE, OR AS DIRECTED BY THE OWNER.
22. TEMPORARY EROSION AND SEDIMENT CONTROL BY OTHERS.
23. TRENCH DRAIN GRAVEL TO BE CRUSHED LIMESTONE, UNIFORMLY GRADED, WITH NO PARTICLE LARGER THAN 3 INCHES AND NO MORE THAN 5% PASSING THE NO. 200 SIEVE. ALSO, SEE NOTE 7 ABOVE AS AN OPTION.

FOUNDATIONS

1. CONTRACTOR TO PROVIDE FOR DE-WATERING OF EXCAVATIONS FROM EITHER SURFACE WATER, GROUNDWATER OR SEEPAGE.
2. CONTRACTOR SHALL PROVIDE FOR DESIGN AND INSTALLATION OF ALL CRIBBING, SHEATHING AND SHORING REQUIRED TO SAFELY RETAIN THE EARTH BANKS.
3. ALL EXCAVATION SHALL BE PROPERLY BACKFILLED. DO NOT PLACE BACKFILL BEHIND RETAINING WALLS. CONCRETE HAS ATTAINED FULL DESIGN STRENGTH. CONTRACTOR SHALL BRACE OR PROTECT ALL BUILDING AND PIT WALLS BELOW GRADE FROM LATERAL LOADS UNTIL ATTACHING FLOORS ARE COMPLETELY IN PLACE AND HAVE ATTAINED FULL DESIGN STRENGTH. CONTRACTOR SHALL PROVIDE FOR DESIGN, PERMITS, AND INSTALLATION OF SUCH BRACINGS.
4. FOUNDATIONS SHALL BEAR ON ROCK, UNDISTURBED, APPROVED, NATIVE SOILS, OR COMPACTED STRUCTURAL FILL. ANY LOOSE MATERIALS SHALL BE COMPACTED OR REMOVED AND REPLACED WITH COMPACTED STRUCTURAL FILL.
5. THE SPECIAL INSPECTOR SHALL ASSESS THE EXPOSED SUBGRADE AT THE BOTTOMS OF ALL EXCAVATIONS - INCLUDING TRENCHES WITHIN THE BUILDING FOOTPRINT. ANY EXISTING UNSUITABLE MATERIAL ENCOUNTERED AT THE SUBGRADE LEVEL SHALL BE OVER-EXCAVATED TO SUITABLE FILL OR NATIVE SOIL. THE OVER-EXCAVATED SOIL SHALL BE REPLACED WITH ON-SITE OR IMPORTED STRUCTURAL FILL.
6. WHERE STRUCTURAL FILL IS USED WITHIN THE BUILDING FOOTPRINT (E.G., BELOW FOOTINGS, SLAB-ON-GRADE, AND BACKFILL FOR UTILITY TRENCHES), IT SHALL BE PLACED IN LIFTS NOT TO EXCEED 8 INCHES AND MECHANICALLY COMPACTED TO 95% OF THE MAXIMUM DRY DENSITY AS MEASURED USING ASTM D-1557. THE SOIL SHALL BE COMPACTED AT MOISTURE CONTENT VALUES WITHIN TWO PERCENTAGE POINTS OF OPTIMUM, AS MEASURED USING ASTM D-1557. FLOODING WILL NOT BE PERMITTED. FROZEN SOILS SHALL NOT BE USED AS FILL OR BACKFILL AND FILL SHALL NOT BE PLACED OVER FROZEN GROUND.
7. DIRECTLY BELOW SPREAD FOOTINGS, PIER FOUNDATIONS, AND SLAB-ON-GRADE, PROVIDE A 6 INCH LAYER OF STRUCTURAL FILL COMPOSED OF WELL GRADED CRUSHED STONE COMPACTED TO AT LEAST 95% OF THE MAXIMUM DRY DENSITY AS MEASURED USING ASTM D-1557.
8. FROST DEPTH IS 24" AT THIS LOCATION. FOUNDATION DEPTHS SHOWN REFLECT THE ASSUMPTION OF ENCOUNTERING BEDROCK BELOW 54".

STRUCTURAL STEEL

1. MATERIAL SHALL MEET THE REQUIREMENTS OF THE FOLLOWING SPECIFICATIONS UNLESS NOTED OTHERWISE:
 STEEL ROLLED CHANNEL AND ANGLE: ASTM A36
 STEEL PLATES AND BARS: ASTM A36
 STEEL PIPE: ASTM A53 GRADE B
 MACHINE BOLTS: A307
 PLAIN HARDENED WASHERS: ASTM F436 TYPE 1
 PLATE WASHERS: A36
 ANCHOR RODS: ASTM F1554 GR. 36, GALVANIZED IN ACCORDANCE WITH ASTM A153.
2. WELDING SHALL BE MADE WITH E70XX ELECTRODES BY QUALIFIED WELDERS.

REINFORCING STEEL

1. REINFORCING BARS SHALL CONFORM TO ASTM A615 GRADE 60. BARS TO BE WELDED SHALL BE ASTM A706.
2. WELDED WIRE FABRIC SHALL CONFORM TO ASTM A-185.
3. DETAIL, FABRICATE, LABEL, SUPPORT AND SPACE ALL CONCRETE REINFORCEMENT IN ACCORDANCE WITH ACI 315, ACI 318 AND THE INTERNATIONAL BUILDING CODE.
4. ALL REINFORCING BAR BENDS SHALL BE MADE COLD.
5. REINFORCING SPLICES SHALL BE AS SHOWN ON THE DRAWINGS OR PER ACI REQUIREMENTS.

CONCRETE

1. CONCRETE MIXES SHALL BE SUBMITTED TO THE STRUCTURAL ENGINEER FOR REVIEW AND APPROVAL.
2. MATERIALS:
 - PORTLAND CEMENT: ASTM C-150, TYPE II, LOW ALKALI
 - AGGREGATE: ASTM C-33
 - AIR ENTRAINMENT: ASTM C260
 - ADMIXTURES: ASTM C494
3. PROPORTION NORMAL WEIGHT CONCRETE MIXES TO ACHIEVE THE FOLLOWING PROPERTIES:
 - MINIMUM COMPRESSIVE STRENGTH: 4,500 PSI AT 28 DAYS
 - MAXIMUM WATER-CEMENTITIOUS RATIO: 0.45
 - AIR CONTENT: 6% PLUS OR MINUS 1.5% AT POINT OF DELIVERY FOR 1 INCH NOMINAL MAXIMUM AGGREGATE

STRUCTURAL NOTES

CONCRETE

4. EXPOSURE CLASSES FOR EXPOSURE CATEGORIES ARE AS FOLLOWS:
 FOUNDATIONS AND SLAB-ON-GRADE
 - FRETZE AND TROWING: F2
 - SULFATE: S1
 - PERMEABILITY: P0
 - CORROSION: C1
5. DESIGN FORMS TO PROVIDE THE SPECIFIED CHAMFERS SHOWN ON THE DRAWINGS. PROTECTING CORNERS SHALL BE FORMED WITH A 3/4" CHAMFER UNLESS OTHERWISE NOTED ON DRAWINGS.
6. ALL REINFORCING BARS, ANCHOR BOLTS AND OTHER CONCRETE INSERTS SHALL BE WELL SECURED IN POSITION PRIOR TO PLACING CONCRETE.
7. PROVIDE SLEEVES FOR PLUMBING AND ELECTRICAL OPENINGS IN CONCRETE BEFORE PLACING. DO NOT CUT ANY REINFORCING WHICH MAY CONFLICT. CORING IN CONCRETE IS NOT PERMITTED EXCEPT AS SHOWN. NOTIFY THE STRUCTURAL ENGINEER IN ADVANCE OF CONDITIONS NOT SHOWN ON THE DRAWINGS.
8. CONDUIT OR PIPE SIZE (O.D.) SHALL NOT EXCEED 30 PERCENT OF SLAB THICKNESS AND SHALL BE PLACED BETWEEN THE TOP AND BOTTOM REINFORCING, UNLESS SPECIFICALLY DETAILED OTHERWISE. CONCENTRATIONS OF CONDUITS OR PIPES SHALL BE AVOIDED EXCEPT WHERE DETAILED OPENINGS ARE PROVIDED.
9. CONCRETE SHALL BE PLACED IN A CONTINUOUS OPERATION UNTIL THE SECTION IS COMPLETE BETWEEN PREDETERMINED CONSTRUCTION JOINTS. CONSTRUCTION JOINTS SHALL BE INSTALLED SO STRENGTH AND APPEARANCE OF CONCRETE ARE NOT IMPAIRED. REINFORCING SHALL CONTINUE ACROSS THE JOINT.
10. AFTER INITIAL FLOATING OF SLAB-ON-GRADE AND PRIOR TO DEVELOPMENT OF RANDOM CONTRACTION CRACKS, PROVIDE CONTROL JOINTS FOR A DEPTH OF MAXIMUM FOUR INCHES AND SLAB THICKNESS AT 18 FEET ON CENTER MAXIMUM BY ONE OF THE FOLLOWING METHODS:
 - SAWCUTTING
 - GROOVING AND FINISHING EACH EDGE OF JOINT TO A RADIUS OF 1/8 INCH.
11. UNLESS NOTED OTHERWISE ON DRAWINGS, MINIMUM CONCRETE COVER OVER REINFORCING SHALL BE 2" ON TOP AND SIDES, AND 3" ON BOTTOM.
12. OPENINGS, POCKETS, ETC., LARGER THAN 6" SHALL NOT BE PLACED IN CONCRETE SLABS, BEAMS, JOISTS, COLUMNS, WALLS, ETC., UNLESS SPECIFICALLY DETAILED ON THE STRUCTURAL DRAWINGS. NOTIFY THE STRUCTURAL ENGINEER WHEN DRAWINGS BY OTHERS SHOW OPENINGS, POCKETS, ETC., LARGER THAN 6" NOT SHOWN ON THE STRUCTURAL DRAWINGS, BUT WHICH ARE LOCATED IN STRUCTURAL MEMBERS.
13. ECC BLOCK AND/OR SIMILAR SHALL BE DESIGNED BY CONTRACTOR. ALSO SEE DWG. 11.

WOOD

1. MATERIALS SHALL MEET THE REQUIREMENTS OF THE FOLLOWING SPECIFICATIONS, UNLESS NOTED OTHERWISE:
 SOLID SAWN SECTIONS: WMPA DOUGLAS FIR-LARCH NO. 1 OR BETTER, EXTERIOR GRADE
 PLYWOOD SHEATHING: STRUCTURAL I SHEATHING EXP. 1 IN ACCORDANCE WITH VOLUNTARY PRODUCT STANDARD PS2.
 NAIL: ASTM F1567 GALVANIZED IN ACCORDANCE WITH A153.
 CONNECTORS: SIMPSON STRONG-TIE WITH ZMAX COATING.
 GLULAM BEAMS: PRODUCED IN ACCORDANCE WITH THE AMERICAN NATIONAL STANDARD FOR WOOD PRODUCTS-STRUCTURAL GLUED LAMINATED TIMBER, ANSI/AITC A190.1. SEE DRAWINGS FOR MEMBER STRESS CLASS. UNLESS SPECIFIED OTHERWISE ON THE DRAWINGS, MEMBERS ARE TO BE FRAMING APPEARANCE CLASSIFICATION.
 METAL PLATE CONNECTED WOOD TRUSSES: TRUSSES SHALL BE DESIGNED AND MANUFACTURED IN ACCORDANCE WITH TRUSS PLATE INSTITUTE'S (TPI'S) NATIONAL DESIGN STANDARD FOR METAL PLATE CONNECTED WOOD TRUSS CONSTRUCTION (ANSI/TPI) AND ALL APPLICABLE LEGAL REQUIREMENTS. METAL CONNECTOR PLATES SHALL BE MANUFACTURED BY A WOOD TRUSS COUNCIL OF AMERICA (WTC) MEMBER PLATE MANUFACTURER AND SHALL NOT BE LESS THAN 20 GAUGE THICKNESS AND SHALL MEET ASTM A563 GRADE 33, AND GALVANIZED IN ACCORDANCE WITH ASTM A624, COATING DESIGNATION 650.
2. EXCEPT WHERE SPECIFIED OTHERWISE ON THE DRAWINGS, MEMBER CONNECTIONS SHALL BE IN ACCORDANCE WITH IBC FASTENING SCHEDULE TABLE 2304.8.1.

WOOD

3. EXCEPT WHERE SPECIFIED OTHERWISE ON THE DRAWINGS, WALLS SHALL BE FRAMED WITH 2x6 STUDS AT 16" CENTERS. WALLS SHALL BE EXTERIOR SHEATHED WITH 3/8" STRUCTURAL I SHEATHING NAILED WITH 8d COMMON AT 6" ALONG PANEL EDGES AND 12" ALONG INTERMEDIATE SUPPORTS. PANEL EDGES TO BE BACKED WITH 2x BLOCKING.
4. ROOFS SHALL BE FRAMED WITH METAL PLATE CONNECTED TRUSSES AT 2 FT. ON CENTERS AND SHEATHED WITH 1" STRUCTURAL I EXP. 1, NAILED WITH 8d COMMON AT 6" CENTERS (UNBLOCKED).
5. COLUMNS FOR FIRING SHEDS SHALL BE PRESERVATIVE TREATED USING WATER-BORNE PRESERVATIVES IN ACCORDANCE WITH AMPA U1 (COMMODITY SPECIFICATIONS A OR P) FOR SOIL AND FRESH WATER USE.
6. WOOD MEMBERS OTHER THAN FIRING SHED COLUMNS NOTED AS PRESERVATIVE TREATED SHALL BE PRESERVATIVE TREATED USING WATER-BORNE PRESERVATIVES IN ACCORDANCE WITH AMPA U1 (COMMODITY SPECIFICATIONS A OR P) FOR ABOVE-GROUND USE.
7. METAL PLATE CONNECTED WOOD TRUSSES SHALL BE DESIGNED FOR DEAD, SNOW, WIND, AND EARTHQUAKE FORCES NOTED ON 'STRUCTURAL DESIGN CRITERIA' OF DRAWING NUMBER 002 AND COMPLY WITH SECTION 2303.4 OF THE 2012 IBC. TRUSS DESIGN DRAWINGS FOR EACH INDIVIDUAL TRUSS SHALL BE STAMPED AND SIGNED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF WASHINGTON AND SUBMITTED TO THE STRUCTURAL ENGINEER FOR ACCEPTANCE AND THE BUILDING OFFICIAL FOR APPROVAL PRIOR TO INSTALLATION.

SPECIAL INSPECTION

- IN ACCORDANCE WITH SECTION 1705 OF THE 2012 IBC, THE OWNER SHALL EMPLOY AN APPROVED TEST AND INSPECTION AGENCY TO PERFORM THE FOLLOWING SPECIAL INSPECTIONS:
1. VERIFICATION THAT EXCAVATIONS FOR FOUNDATIONS ARE TO PROPER SIZE AND DEPTH AND THAT EXPOSED SUBGRADE IS ADEQUATE TO ACHIEVE THE DESIGN BEARING CAPACITY.
 2. CLASSIFICATION AND TESTING OF COMPACTED FILL MATERIALS AND VERIFICATION OF USE OF PROPER MATERIALS, DENSITIES, AND LIFT THICKNESS DURING PLACEMENT AND COMPACTION OF COMPACTED FILL.
 3. ALL CAST-IN-PLACE CONCRETE FOR FOUNDATIONS, INCLUDING PIERS AND SPREAD FOOTINGS.
 4. BOLTS INSTALLED IN CONCRETE WHICH INCLUDES, BUT NOT LIMITED TO, COLUMN ANCHOR BOLTS.
 5. PLACING OF REINFORCING STEEL.
 6. EPOXY ANCHOR BOLTS.
 7. WELDS IDENTIFIED BY TABLE 1705.2.2 OF 2012 IBC AS REQUIRING CONTINUOUS OR PERIODIC SPECIAL INSPECTION NEED NOT HAVE SPECIAL INSPECTION WHEN THE WELDING IS DONE IN AN APPROVED FABRICATOR'S SHOP. WHEN WELDING IS DONE IN A SHOP WHICH IS NOT AN APPROVED FABRICATOR, SPECIAL INSPECTION IS REQUIRED AND AN APPLICATION TO PERFORM OFF-SITE FABRICATION MUST BE SUBMITTED TO AND APPROVED BY BUILDING OFFICIAL.
 8. EXPANSION (WEDGE) ANCHOR BOLTS.

GENERAL NOTES

1. ALSO SEE NOTES AND SPECIFICATIONS ON DRAWING NO. 02

TOTAL SITE CUT/FILL VOLUMES***

TOTAL FILL VOLUME	XXXXX CY
TOTAL CUT VOLUME	XXXXX CY
NET FILL VOLUME	XXXXX CY

*** CUT/FILL VOLUMES ESTIMATED ASSUMES A ±10% CONTINGENCY

PHASE 1 AND 2 CUT/FILL VOLUMES***

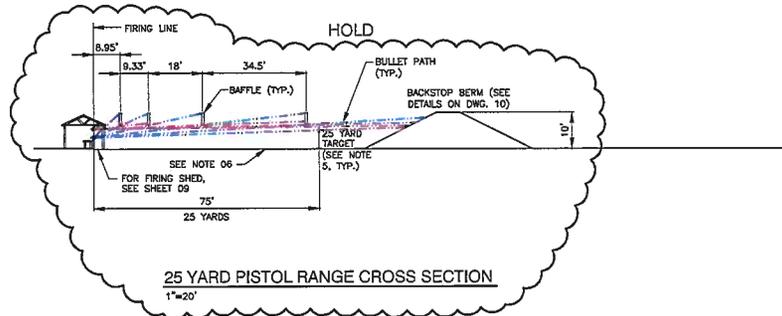
TOTAL FILL VOLUME	XXXXX CY
TOTAL CUT VOLUME	XXXXX CY
NET FILL VOLUME	XXXXX CY

*** CUT/FILL VOLUMES ESTIMATED ASSUMES A ±10% CONTINGENCY

<p style="text-align: center;">PRELIMINARY NOT FOR CONSTRUCTION</p>				JOB No. 60484412	DESIGNED: JLP	PROJ. ENGINEER: SK		DAKOTA COUNTY, MN	SOUTH SAINT PAUL ROD & GUN CLUB PROJECT	DRAWING NUMBER: 05
				SCALE: AS NOTED	DRAWN BY: JG	APPROVED BY: SK				CAD FILE NUMBER: D5-civil notes
B 06/02/18 SK FOR FINAL REVIEW				CHECKED BY: MLT	DATE: JUNE 2016	111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com	600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	CIVIL AND STRUCTURAL NOTES	SHEET: 5 of 13	REV. B

User: rater: ronus - P:\hotels: Jun 01, 2016 - 9:32am
 Last: Some: Jun 01, 2016 - 9:28am
 J:\0484412 South St. Paul ROD SANV000_CAD\05-Sheets\05-civil notes.dwg

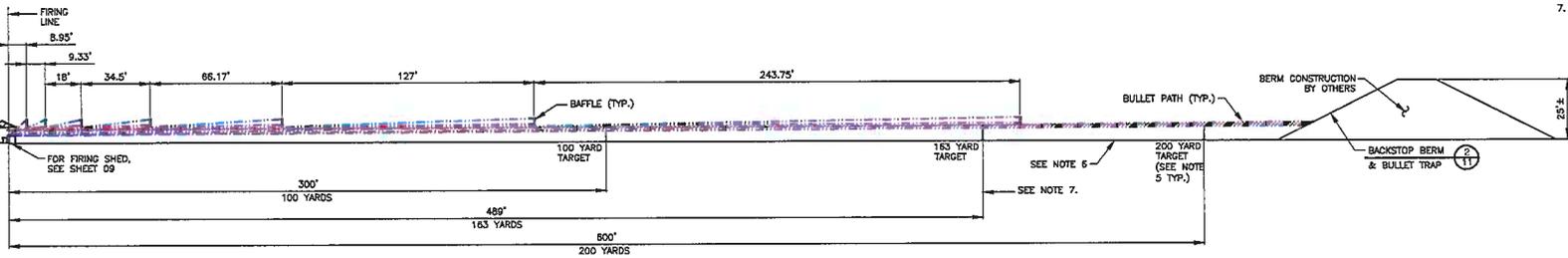
j:\0484412 South St. Paul ROD SAR\000_CAD\02-Sheets\06-Firing.dwg User:palmer,renus Plotted:Jun 02, 2016 - 7:51am Last Saved:Jun 02, 2016 - 7:51am



LEGEND:

- HIGHEST FIRING POSITION BULLET PATH
- LOWEST FIRING POSITION BULLET PATH

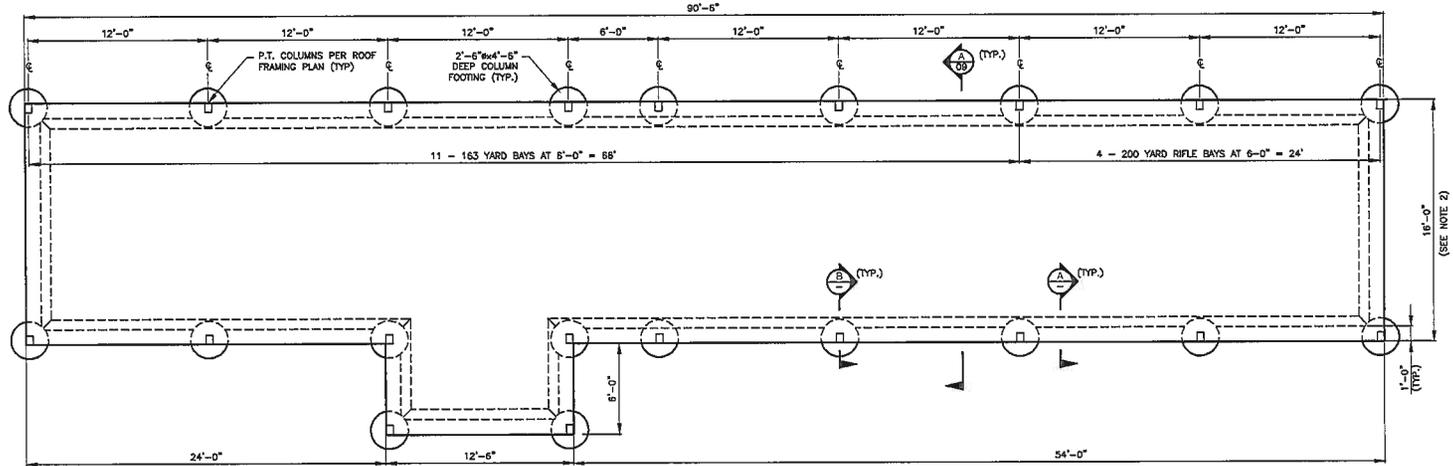
- NOTES:**
1. FOR BAFFLE DETAILS, SEE SHEET 09.
 2. ONLY CLOSEST THREE BAFFLES TO FIRING SHED TO BE ARMORED, SEE SHEET 09 FOR DETAILS.
 3. BAFFLE LOCATIONS SHOWN ARE MAXIMUM DIMENSIONS, TO ACHIEVE HIGHEST BULLET STRIKE POINT OF 3 INCHES BELOW TOP OF BAFFLE.
 4. DIMENSIONS SHOWN ARE TO CENTERLINE OF BAFFLE AND TARGET COLUMN.
 5. TARGETS PROVIDED BY OWNER.
 6. SEE GRADING ON DWG. 03 AND RELATED NOTES ON DWG. 02.
 7. BACKSTOP BERM IS NOT SHOWN HERE AS SECTION IS FOR 200 YARD RANGE. THERE IS A SIMILAR BACK STOP BERM FOR THE 163 YARD RANGE. SEE DWG. 04 FOR LOCATION. CONSTRUCT USING DETAILS SHOWN ON DWG'S. 10, 11, & 12. TOP OF BERM FOR 163 YARD RANGE IS 10'.



200 YARD RIFLE RANGE CROSS SECTION
1"=30'

PRELIMINARY NOT FOR CONSTRUCTION				JOB No. 60484412		DESIGNED: PS		PROJ. ENGINEER: SK				DAKOTA COUNTY, MN		SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT		DRAWING NUMBER: 06			
				SCALE: AS NOTED		DRAWN BY: JG		APPROVED BY: SK								111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com		600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	
B 06/02/16		SK		FOR FINAL REVIEW		CHECKED BY: MLT		DATE: JUNE 2016								SHEET: 6 of 13		REV. B	
No.		DATE		BY		REVISION													

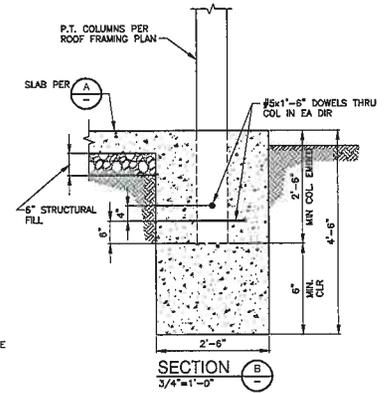
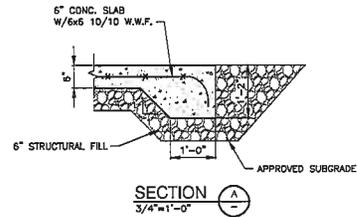
J:\P0484412 South St. Paul ROD SAN V000_CAD\02-Sheets\07-concrete.dwg User:skaterrenus Plot:Sat, Jun 01, 2016 9:25am Last Saved: May 31, 2016 5:04pm



CONCRETE OUTLINE FOR RIFLE/PISTOL FIRE LINE SHED
1/4"=1'-0"

NOTES:

1. SEE STRUCTURAL NOTES ON DRAWING NUMBER 5 FOR INFORMATION REGARDING FOUNDATIONS, CONCRETE REINFORCING SPLICES, CONTROL JOINTS, AND CONSTRUCTION JOINTS.
2. GAP FOR CONCRETE EDO-BLOCK WALL SEE SITE DEVELOPMENT PLAN, SHEET 04 FOR WALL LOCATION.



PRELIMINARY NOT FOR CONSTRUCTION			
B 06/02/16 SK FOR FINAL REVIEW		REVISION	
No.	DATE	BY	REVISION

JOB No. 60484412	DESIGNED: JLP	PROJ. ENGINEER: SK
SCALE: AS NOTED	DRAWN BY: PCF	APPROVED BY: SK
CHECKED BY: MLT	DATE: JUNE 2016	

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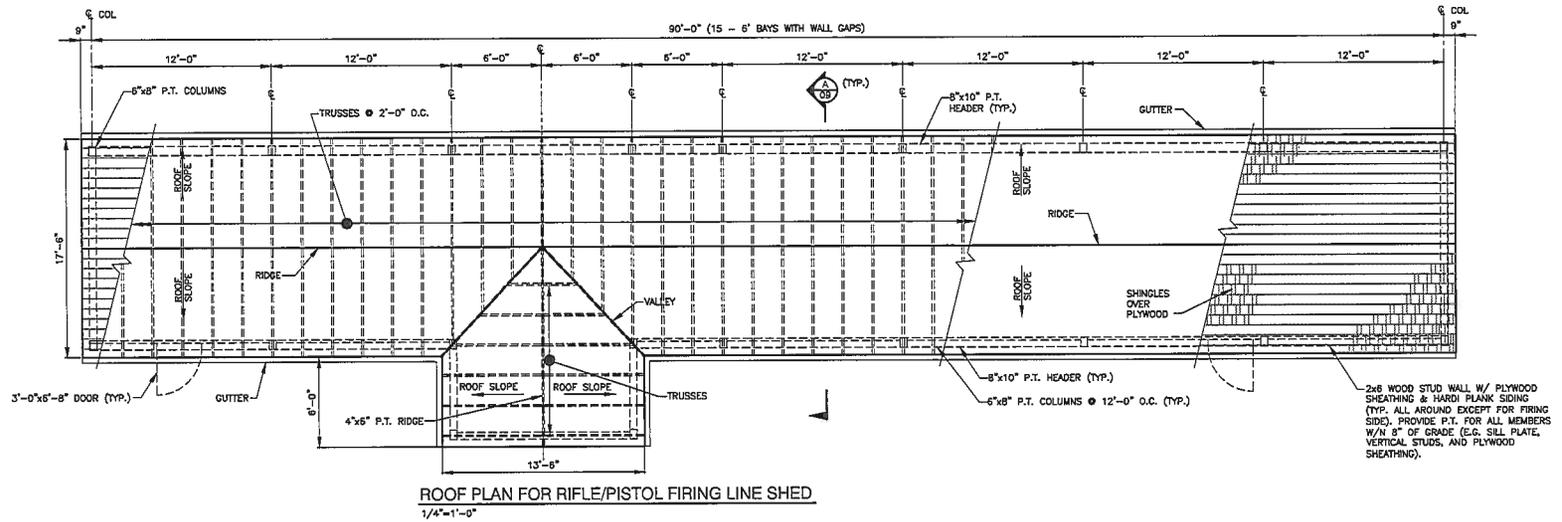
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DAKOTA COUNTY, MN
**600 GUN CLUB ROAD
SAINT PAUL, MINNESOTA**

SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT
**FIRING LINE SHED
FOUNDATION PLAN**

DRAWING NUMBER: 07	
CAD FILE NUMBER: 07-concrete	
SHEET: 7 of 13	REV. B

J:\0684412\South St. Paul ROD SAR\000_CAD\02-Sheets\08-roof.dwg User:pteter.renus Plotted:Jun 01, 2018 -- 9:27am Last Saved:May 31, 2018 -- 5:04pm

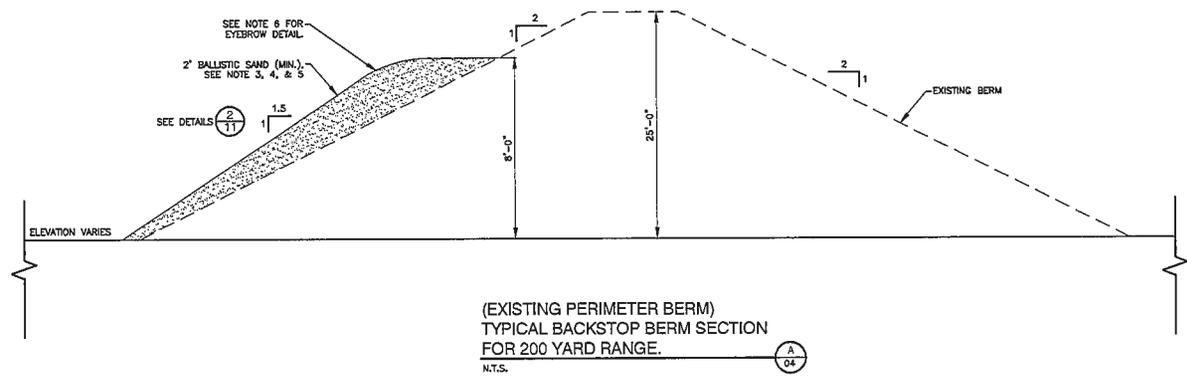


NOTES:

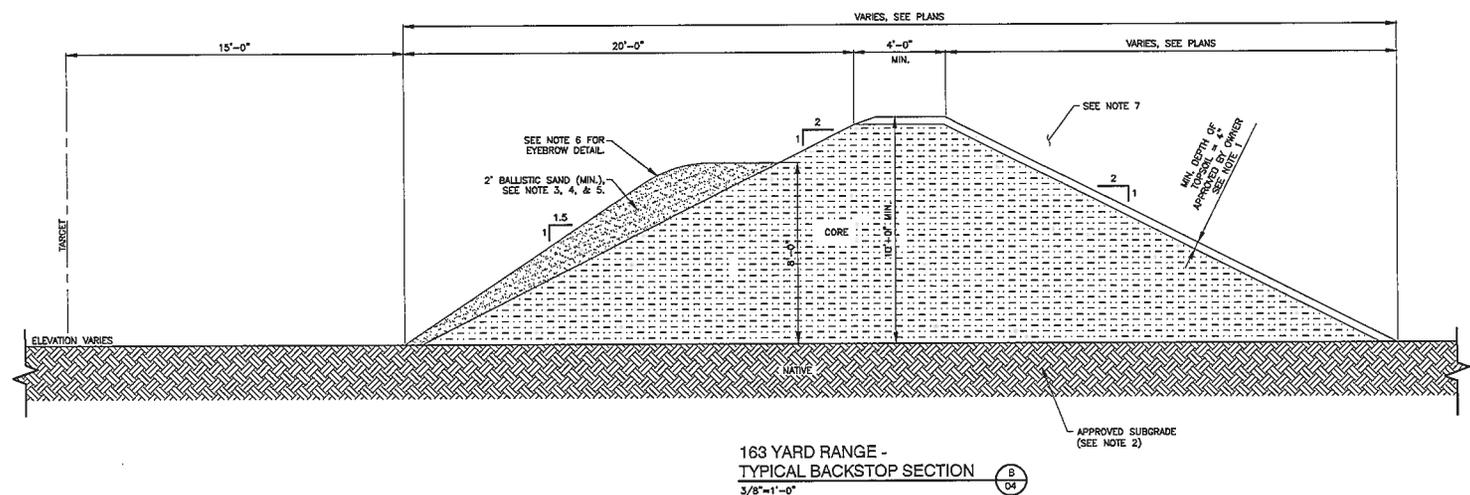
1. SEE STRUCTURAL NOTES ON DRAWING NUMBER 5 FOR INFORMATION REGARDING WOOD FRAMED CONSTRUCTION.
2. GUTTERS TO BE 2" SEAMLESS ALUMINUM GUTTERS, 0.027" (MIN.) IN THICKNESS.
3. GUTTERS TO SLOPE 1" (MIN.) IN 40'. USE RING SHANK SPIKE OR HIDDEN HANGERS WITH SCREWS TO SECURE AT 30" (MAX.) SPACING.
4. DOWNSPOUTS TO BE 0.019" (MIN.) IN THICKNESS AND 2"x3" (MIN.) AND SECURED TO SIDING OR COLUMN.
5. INSTALL DOWNSPOUTS AT EACH CORNER AND AT 30' INTERVALS (MAX.) IN LINE WITH COLUMN. DISCHARGE ONTO SPLASH BLOCKS.
6. INSTALL GUTTERS AND DOWNSPOUTS PER MANUFACTURER'S RECOMMENDATIONS.

PRELIMINARY NOT FOR CONSTRUCTION				JOB No. 60484412	DESIGNED: JLP	PROJ. ENGINEER: SK	 111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4282 www.aecom.com	DAKOTA COUNTY, MN 600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT FIRING LINE SHED ROOF FRAMING PLAN	DRAWING NUMBER: OB	
				SCALE: AS NOTED	DRAWN BY: PCF	APPROVED BY: SK				CAD FILE NUMBER: 08-roof	
				CHECKED BY: MLT	DATE: JUNE 2015	SHEET: 8 of 13				REV. B	
B	06/22/18	SK	FOR FINAL REVIEW								
No.	DATE	BY	REVISION								

i:\01949412 South St. Paul ROD SAN\000_CAD\02-Sheets\0-Berm_aset.dwg User:pteter.namus Plotted:Jun 02, 2016 - 9:47am

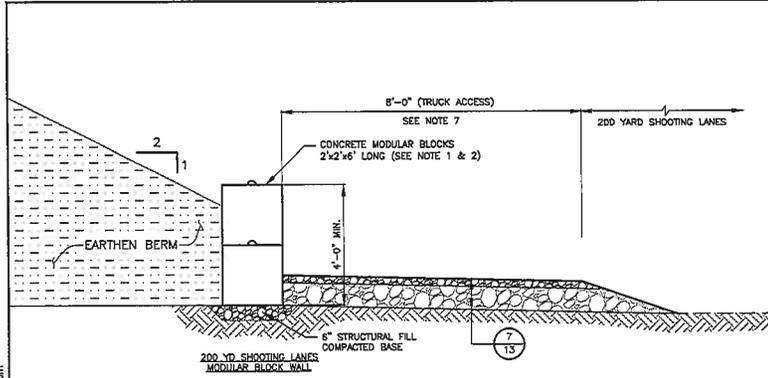


- NOTES:**
1. VEGETATE BERM WITH APPROVED SEED MIX.
 2. SUBGRADE TO BE STRIPPED OF UNSUITABLE MATERIALS AND PROOF ROLLED UNDER THE OBSERVATION OF A QUALIFIED INSPECTOR PRIOR TO PLACING BACKFILL.
 3. TREAT BALLISTIC SAND WITH LIME TO ACHIEVE SOIL PH BETWEEN 5.5 - 6.5.
 4. SEE DRAWING NO.'S 02 & 05 FOR NOTES AND SPECIFICATIONS SEE DWG NO. 04 FOR BERM LAYOUT.
 5. ALL GROUND Baffles AND/OR INTERMEDIATE BERMS SHALL BE CONSTRUCTED WITH BALLISTIC SAND, UNLESS OTHERWISE DIRECTED BY OWNER CONSTRUCT USING SIMILAR DETAILS AND MATERIALS.
 6. SEE DWG. 12 FOR EYEBROW DETAIL.
 7. SEE DWG.'S. 04 AND 11 FOR CONSTRUCTION OF ECO BLOCK WALL ON WEST END OF BACKSTOP BERM. TOP OF WALL TO BE MINIMUM 6" ABOVE TOP OF BACKSTOP BERM.

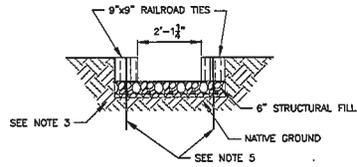


PRELIMINARY NOT FOR CONSTRUCTION				JOB No. 00484412	DESIGNED: PS	PROJ. ENGINEER: SK		DAKOTA COUNTY, MN	SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT	DRAWING NUMBER: 10	
				SCALE: AS NOTED	DRAWN BY: JG	APPROVED BY: SK				CAD FILE NUMBER: 10-Berm	
B	06/02/16	SK	FOR FINAL REVIEW	CHECKED BY: MLT	DATE: JUNE 2016		111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com	600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	BERM SECTIONS	SHEET: 10 of 13	REV. B
No.	DATE	BY	REVISION								

J:\60484412 South St. Paul ROD SAN\000_CAD\02-Sheets\CV11-Mod_det.dwg User:epeterremus Plotted:Jun 02, 2016 - 9:53am Last Save:Jun 02, 2016 - 9:51am

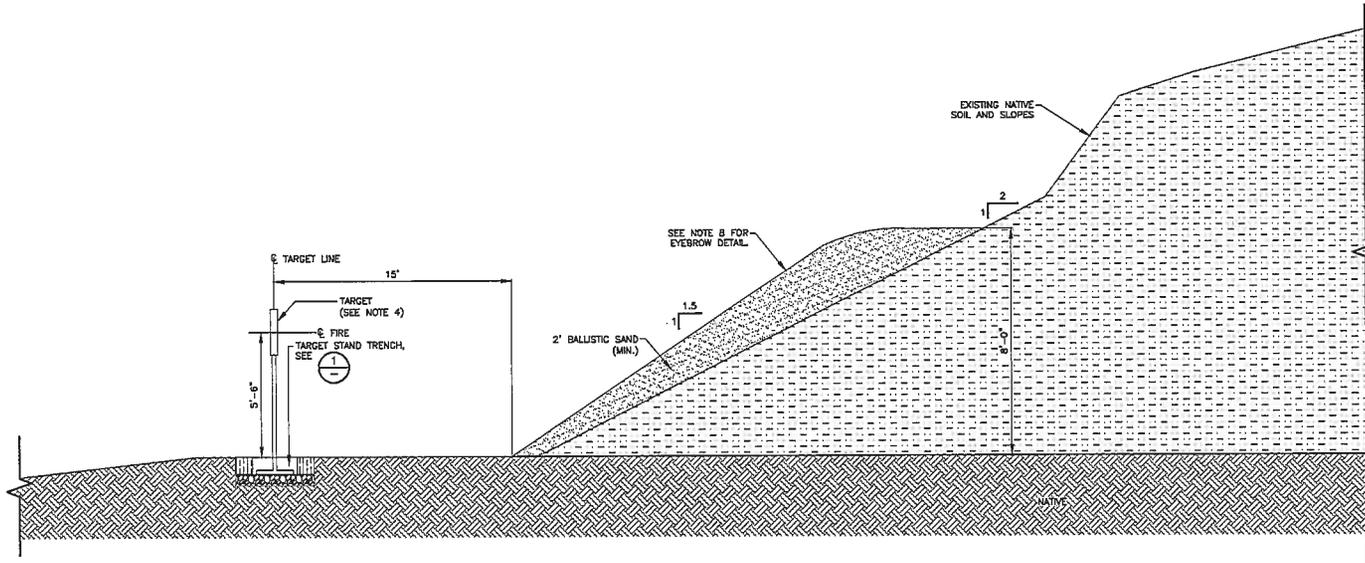


SECTION C
1/2"=1'-0"



TARGET STAND TRENCH DETAIL
1/2"=1'-0" (SEE NOTE 4)

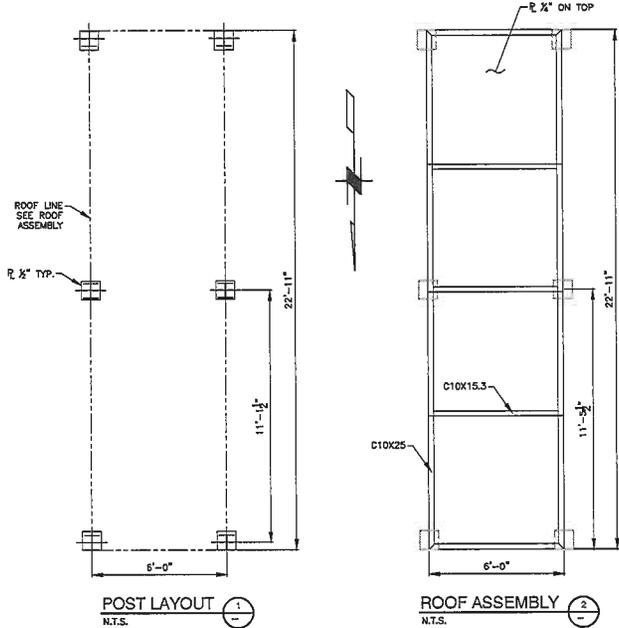
- NOTES:**
1. INSTALL CONCRETE MODULAR BLOCK WALLS (ECOLOGY, ECO, OR ULTRA BLOCK OR EQUAL FROM LOCAL SUPPLIER) IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDED CONSTRUCTION AND INSTALLATION GUIDELINES. INDIVIDUAL BLOCK MAY VARY. MANUFACTURER TO CERTIFY FOR INTENDED USE ON PROJECT.
 2. EXAMPLE CONCRETE MODULAR BLOCK CONFIGURATION SHOWN. FINAL CONFIGURATION TO BE DETERMINED BY THE OWNER.
 3. SUBGRADE TO BE STRIPPED OF UNSUITABLE MATERIALS AND PROOF ROLLED UNDER THE OBSERVATION OF A QUALIFIED INSPECTOR PRIOR TO PLACING BACKFILL.
 4. TARGET SUPPLIED BY OWNER.
 5. SECURE RAILROAD TIES AT EACH END OF THE TIE WITH #6 REBAR EMBEDDED 18" BELOW TIE.
 6. SEE NOTES AND SPECIFICATIONS ON DRAWING No. 02.
 7. THE INTENT OF THE ECO BLOCK IS TO ALLOW A STANDARD LONG BED PICKUP ACCESS AROUND THE END OF THE FIRING LINE SHED.
 8. SEE DWG. 12 FOR EYEBROW DETAIL.



200 YARD BULLET TRAP
3/8"=1'-0"

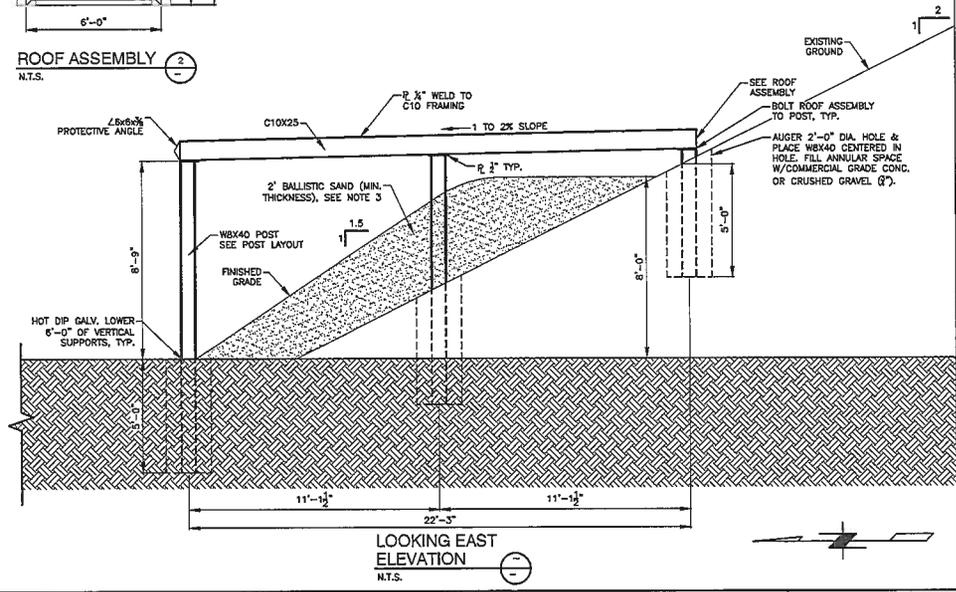
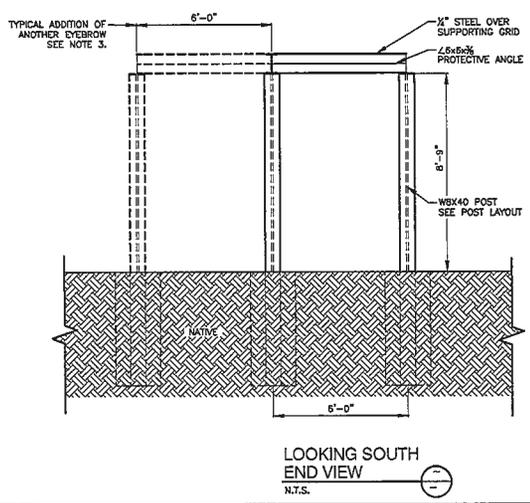
PRELIMINARY NOT FOR CONSTRUCTION				JOB No. 60484412 SCALE: AS SHOWN		DESIGNED: PS DRAWN BY: JG CHECKED BY: MLT		PROJ. ENGINEER: SK APPROVED BY: SK DATE: JUNE 2016			DAKOTA COUNTY, MN 600 GUN CLUB ROAD SAINT PAUL, MINNESOTA	SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT ECOLOGY BLOCK WALL & BULLET TRAP SECTIONS AND DETAILS		DRAWING NUMBER: 11	
				111 SW Columbia, Suite 1500 Portland, Oregon 97201-5814 (tel) 503-222-7200 (fax) 503-222-4292 www.aecom.com		SHEET: 11 of 13 REV: B									

J:\P0484412 South St. Paul ROD SAR\000_CAD\02-Sheets\C12-Eyebrow.dwg User:peker.nemus Plotted:Jun 02, 2016 - 9:55am Plot Size:Jun 02, 2016 - 9:55am



MATERIALS LIST - ONE BAY					
MEMBER	TYPE	LENGTH (ft)	WIDTH (ft)	TOTAL LENGTH (ft) or TOTAL AREA	WEIGHT (lbs)
POST	WBX40	-	-	61.5	2,460
BASE PLATE	R 1/2"	1	1	6	733
ROOF STRINGER	C10X25	-	-	45.84	1,145
ROOF DIAPHRAGM	C10X15.3	-	-	30	459
ROOF PLATE	R 1/2"	22.92	6	137.52	1,404
PROTECTIVE ANGLE	L6X6X3/8	-	-	6	89
TOTAL STEEL WEIGHT (lbs)					6,293

- NOTE:**
1. CONCEPT SHOWN IS FOR 1 SHOOTING BAY ONLY. MULTIPLE SHOOTING BAYS CAN BE CONSTRUCTED BY ADDING POSTS AND ROOF ASSEMBLY UNITS.
 2. FINAL CONSTRUCTION DETAILS TO BE DESIGNED BY CONTRACTOR TO MEET THE LOCAL LOADING AND BUILDING PERMIT REQUIREMENTS.
 3. FOR THIS INSTALLATION, INSTALL 15 EYEBROWS.



**PRELIMINARY
NOT FOR CONSTRUCTION**

JOB No. 60484412	DESIGNED BY GC	PROJ. ENGINEER MLT
SCALE AS NOTED	DRAWN BY JG	APPROVED BY SK
CHECKED BY MLT	DATE JUNE 2016	

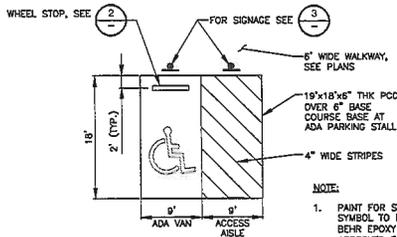
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DAKOTA COUNTY, MN
**600 GUN CLUB ROAD
SAINT PAUL, MINNESOTA**

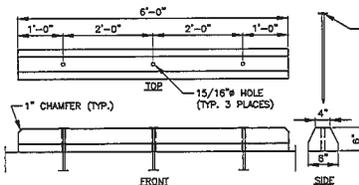
SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT
CONCEPT RIFLE EYEBROW

DRAWING NUMBER: 12
CAD FILE NUMBER: 12-Eyebrow
SHEET: 12 of 13
REV. B

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ADA PARKING STALL AND ENLARGED SIGNAGE PLAN
N.T.S.



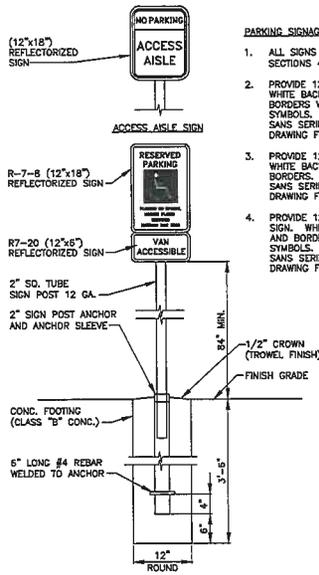
RECYCLED PLASTIC WHEEL STOP DETAIL
N.T.S.



NO TRESPASSING SIGN
N.T.S. (SEE NOTE 5)



DANGER SIGN
N.T.S. (SEE NOTE 5)



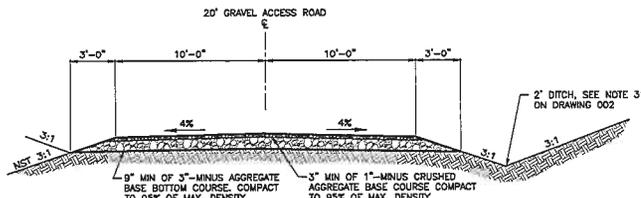
ADA PARKING / ACCESS AISLE SIGNAGE
N.T.S.

PARKING SIGNAGE NOTES:

- ALL SIGNS SHALL COMPLY WITH ADAAG SECTIONS 4.1.2(7), 4.1.2(7)(6), & 4.1.2(7)(b).
- PROVIDE 12"x18" "RESERVED PARKING" SIGNS. WHITE BACKGROUND, GREEN LETTERS AND BORDERS WITH BLUE SQUARE AND WHITE SYMBOLS. LETTERS SHALL BE UPPER CASE, SANS SERIF OR SIMPLE SERIF TYPE. SEE DRAWING FOR LOCATIONS.
- PROVIDE 12"x6" "VAN ACCESSIBLE" SIGNS. WHITE BACKGROUND, GREEN LETTERS AND BORDERS. LETTERS SHALL BE UPPER CASE, SANS SERIF OR SIMPLE SERIF TYPE. SEE DRAWING FOR LOCATIONS.
- PROVIDE 12"x18" "PASSENGER LOADING ZONE" SIGN. WHITE BACKGROUND, GREEN LETTERS AND BORDERS WITH BLUE SQUARE AND WHITE SYMBOLS. LETTERS SHALL BE UPPER CASE, SANS SERIF OR SIMPLE SERIF TYPE. SEE DRAWING FOR LOCATIONS.

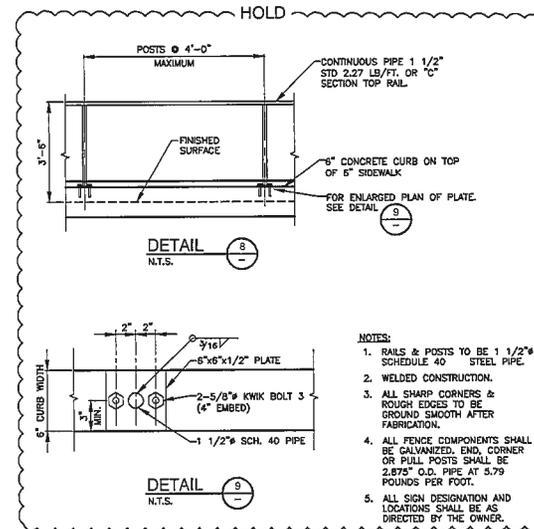
ADAAG COMPLIANCE:

- INTERNATIONAL SYMBOL OF ACCESSIBILITY PER ADAAG SECTIONS 4.1.2 (7)(a).
- VAN ACCESSIBLE PARKING SPACE AND ACCESS AISLE (PER ADAAG SECTIONS 4.6.3, 4.6.4 & 4.6.6): 2% MAX. SLOPE IN ANY DIRECTION.
- ACCESSIBLE ROUTE (PER ADAAG SECTION 4.3): SLOPE SHALL NOT EXCEED 1:20. CROSS SLOPE SHALL NOT EXCEED 1:50. MAX. LEVEL CHANGE: 1/4".



- NOTES:**
- FILL AND CUT SLOPES SHALL NOT BE STEEPER THAN (NST) 3:1 UNLESS OTHERWISE APPROVED BY THE COUNTY.
 - FIRING RANGE ACCESS ROADS TO BE 8' WIDE USING SAME 12" SECTION.
 - ALL SIGN DESIGNATION AND LOCATIONS SHALL BE AS DIRECTED BY THE OWNER.

ACCESS ROAD DETAIL
N.T.S.



PRELIMINARY NOT FOR CONSTRUCTION			
NO.	DATE	BY	REVISION
A	06/02/16	SK	FOR FINAL REVIEW

JOB No.	DESIGNED:	PROJ. ENGINEER:
60484412	JLP	SK
SCALE:	DRAWN BY:	APPROVED BY:
AS NOTED	PCF	SK
CHECKED BY:	DATE:	
MLT	JUNE 2016	



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DAKOTA COUNTY, MN

**600 GUN CLUB ROAD
SAINT PAUL, MINNESOTA**

SOUTH SAINT PAUL ROD AND GUN CLUB PROJECT

CIVIL DETAILS

DRAWING NUMBER:	13
CAD FILE NUMBER:	13-details
SHEET:	13 of 13
REV.	A



COUNCIL WORKSESSION REPORT

DATE:

DEPARTMENT: **Engineering**

ADMINISTRATOR: SPK

6

AGENDA ITEM: Update and Grant Application Strategies for Concord Street Improvements, Wentworth Sidewalk Improvements, and Southview Boulevard & 3rd Avenue Improvements

ACTION TO BE CONSIDERED:

Information item only; staff is requesting input and direction.

OVERVIEW:

On February 11, 2016, the Metropolitan Council Transportation Advisory Board (TAB) adopted the criteria and measures for the biennial regional solicitation for federal transportation projects for program years 2020 and 2021. The Regional Solicitation is part of Metropolitan Council's required distribution of funding per the recently authorized Federal Transportation Act.

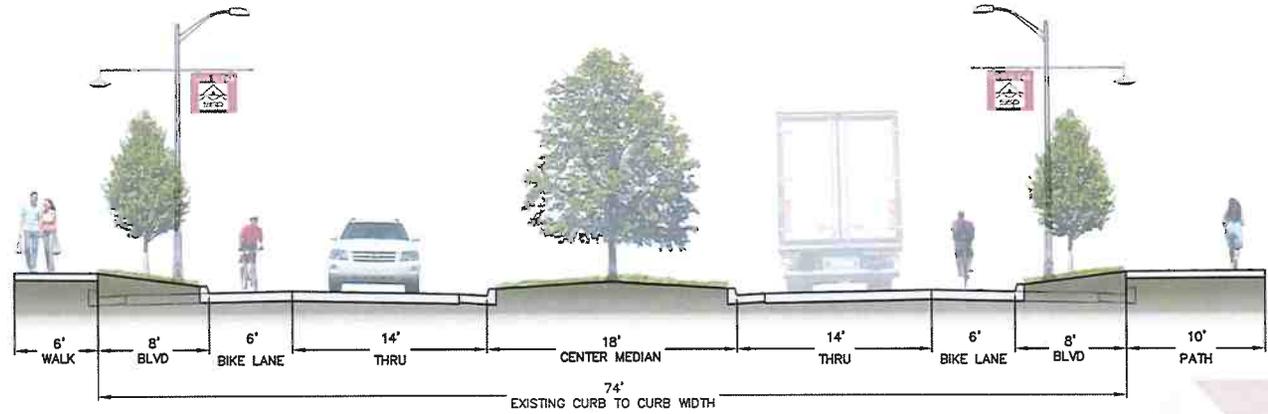
Staff believes that the 2016 regional solicitation is the best opportunity to obtain significant funding for several projects in the City's Capital Improvement Plan (CIP) including the Concord Street Improvements, Wentworth Sidewalk Improvements, and Southview Boulevard and 3rd Avenue South (Southview Boulevard) Improvements. The maximum federal award for the Concord Street Improvements is \$7,000,000 and \$1,000,000 for the Wentworth Sidewalk and Southview Boulevard Improvements. All regional solicitation projects require a 20 percent local match from non-federal sources. Staff will present an update on the status for the projects and discuss strategies for the grant application submittals at the June 27, 2016 City Council work session meeting.

TIMELINE AND SCHEDULE:

On July 5, 2016, staff will request that the City Council consider authorizing staff to provide a letter of support for projects to be submitted for the 2016 regional solicitation. The 2016 regional solicitation grant application deadline for submittals is July 15, 2016. Staff anticipates that the Transportation Advisory Board (TAB) will award successful projects near the end of 2016.

SOURCE OF FUNDS:

No fiscal impact at this time.



Concord Street

— PLANNING STUDY —

COUNCIL WORK SESSION
JUNE 27, 2016



Kimley»Horn



PURPOSE OF STUDY

- ▶ Develop a Vision for Concord Street
- ▶ Identify Funding Sources/Opportunities
- ▶ Gather Feedback from City Leadership and Stakeholders
- ▶ Establish Implementation Plan

CONCORD STREET

▶ **Section A**

▶ 1-494 to Wentworth Avenue

▶ **Section B**

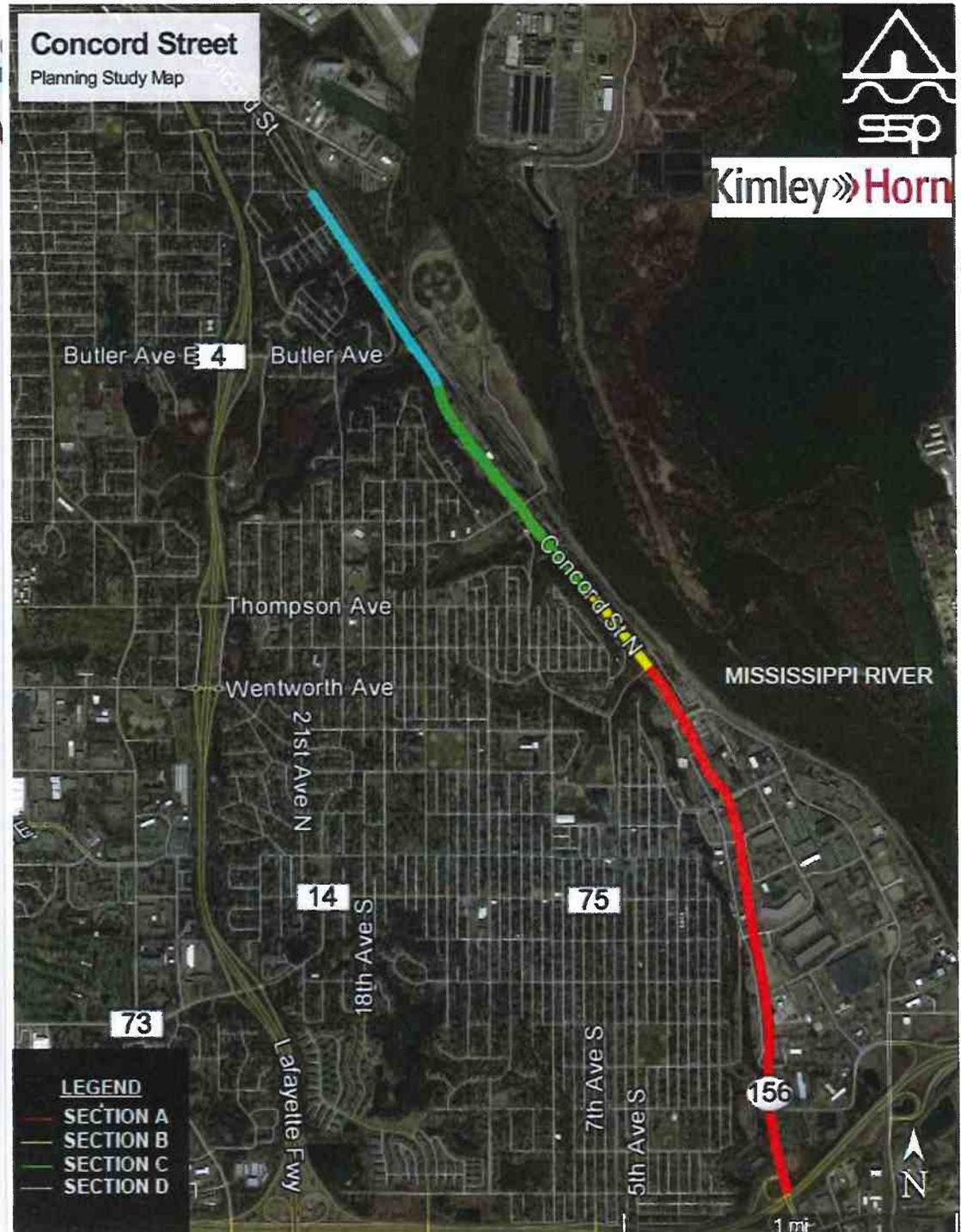
▶ Wentworth Avenue to Fury Motors

▶ **Section C**

▶ Fury Motors to Cenex Office Building

▶ **Section D**

▶ Cenex Office Building to Annapolis Street

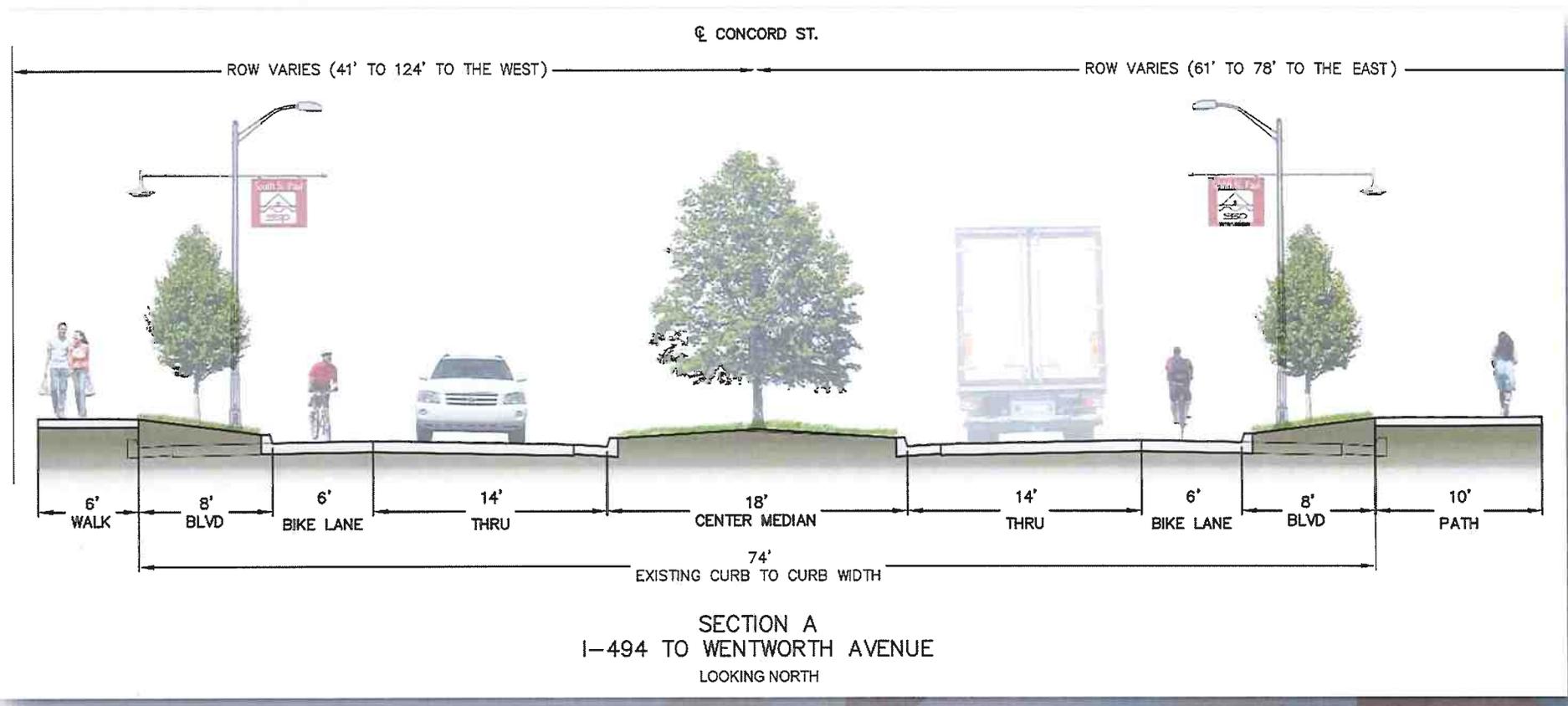




Proposed Typical Section

► Section A

- Reduce from 4-lane to 2-lane, maintain turn lanes and widths
- Add bike lanes, off-road path & sidewalk
- Landscaped medians/boulevards where possible

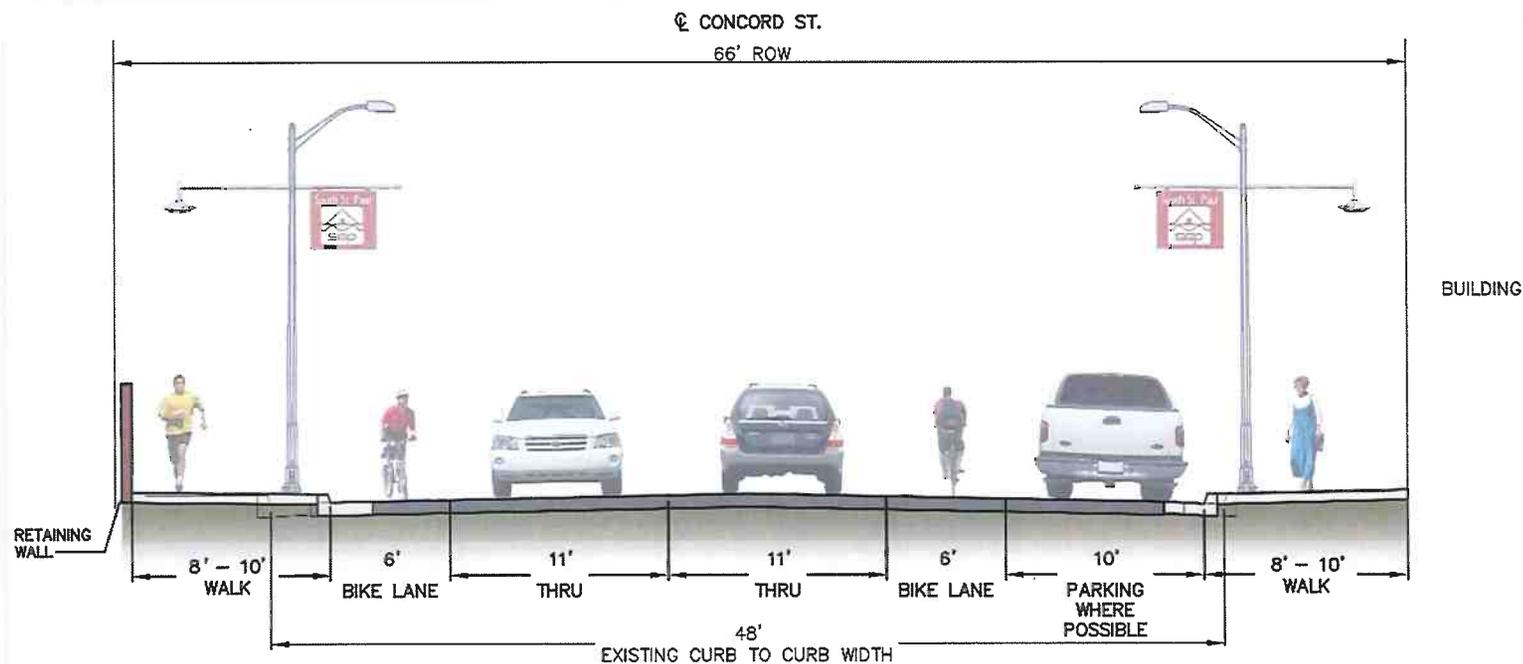




Proposed Typical Section

► Section B

- Maintain 2-lane section with narrower lane widths
- Add bike lanes & sidewalks
- Maintain on-street parking on east side where possible
- Replace retaining walls as necessary



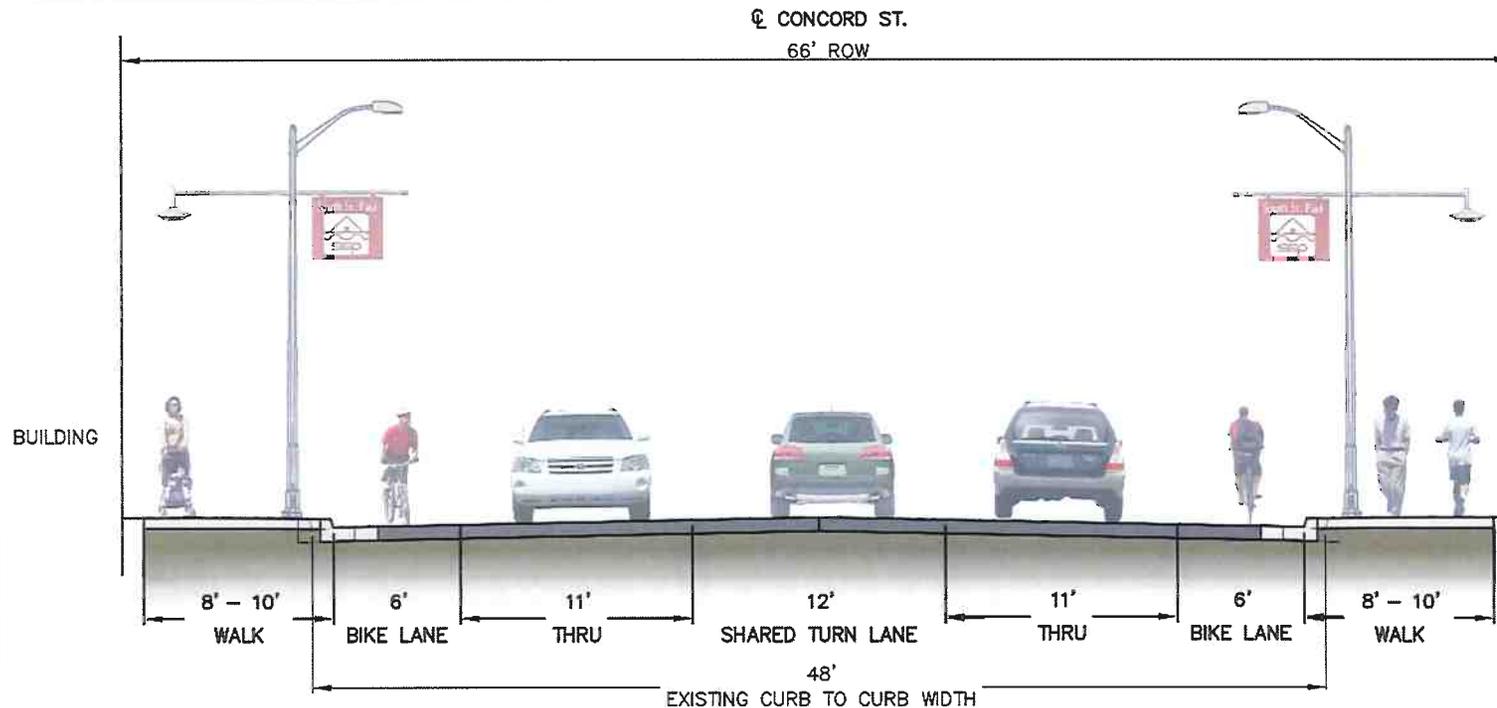
SECTION B
WENTWORTH AVENUE TO
STA. 208+00 (FURY MOTORS)
LOOKING NORTH



Proposed Typical Section

► Section C

- Convert to 3-lane section with shared center turn lane
- Add bike lanes & sidewalks
- Remove on-street parking from both sides, explore areas for parking bump-outs and off street parking where necessary



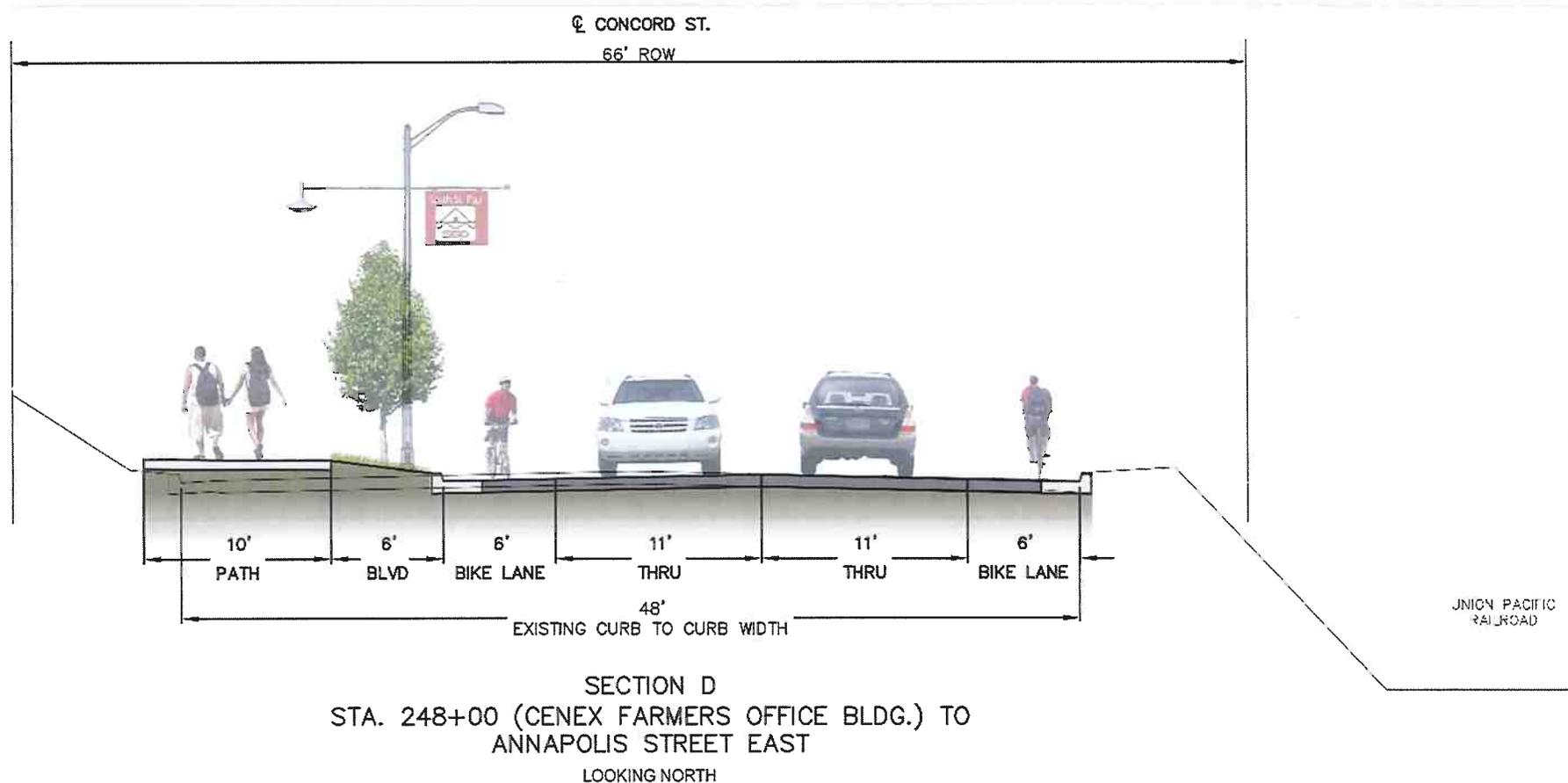
SECTION C
SECTION C STA. 208+00 (FURY MOTORS) TO
STA. 248+00 (CENEX FARMERS OFFICE BLDG.)
LOOKING NORTH



Proposed Typical Section

▶ Section D

- ▶ Maintain 2-lane section, turn lanes where necessary
- ▶ Add bike lanes and off-road path on west side (away from RR)
- ▶ Provide path connectivity to Simon's Ravine and MRT
- ▶ Maintain on-street parking in parking bays where necessary





PUBLIC ENGAGEMENT - PROCESS

- ▶ Project Newsletter
 - ▶ Sent to 440 property owners
 - ▶ Introduce project and notification for open house

- ▶ City Website
 - ▶ Includes all mailings and materials shown at the open house
 - ▶ Link to website was included on newsletter

- ▶ Public Open House
 - ▶ Held May 26, 2016
 - ▶ Representation from 15 properties, 20-25 total attendees
 - ▶ Requested input on overall project, safety, on-street parking, bike/pedestrian facilities, and landscaping aesthetics



PUBLIC ENGAGEMENT – COMMENTS RECEIVED

- ▶ Overall Project
 - ▶ Strong support to improve Concord Street
 - ▶ Generally supportive of the overall vision
 - ▶ Some opposition to reducing lanes in Section A
- ▶ Safety
 - ▶ Vehicles exceeding posted speed limits
 - ▶ No dedicated area for bicycles
 - ▶ Lack of consistent pedestrian facilities
- ▶ Bikes/Pedestrian Facilities
 - ▶ Majority of comments in favor of on-road bikes lanes
 - ▶ Strong support for sidewalks & bike facilities
 - ▶ Consideration of Hardman Ave as the bike route within Section A



PUBLIC ENGAGEMENT – COMMENTS RECEIVED

- ▶ On-Street Parking
 - ▶ General support for on-street parking in Sections A/B/D
 - ▶ Mixed reaction to removing on-street parking on Section C.
 - ▶ Develop parking bays within specific areas of Section C.
- ▶ Landscaping/Aesthetics
 - ▶ Wide spread support for landscaping/aesthetic upgrades
 - ▶ Support focused more on trees/plantings for easier maintenance
 - ▶ Support for replacing retaining walls in Section B with consistent look.



PROJECT IMPLEMENTATION

- ▶ Option 1: I-494 to Annapolis Street (Sections A thru D)
- ▶ Option 2: Hardman Ave to Annapolis St (Sections B thru D)
 - ▶ Requires striping improvements south of Wentworth

	Option 1: I-494 to Annapolis St	Option 2: Hardman Ave to Annapolis St
Pros	<ul style="list-style-type: none"> • Addresses bike/pedestrian connectivity through entire corridor • Higher grant scoring potential 	<ul style="list-style-type: none"> • Lower Project Cost (\$11m total, \$1.5m City) • Addresses area of most concern • More public support for proposed improvements
Cons	<ul style="list-style-type: none"> • Higher Project Cost (\$19m total, \$5m City) • Public concern for roadway narrowing and changes to Section A 	<ul style="list-style-type: none"> • Does not address bike/pedestrian connectivity south of Wentworth (parallel routes exist) • Competitive grant application, but lower potential than Option 1
Staff Recommendation	<ul style="list-style-type: none"> ➤ Option 1 not recommended for initial phase of project implementation ➤ Continue to work with MnDOT on pavement rehab project and other future funding opportunities 	<ul style="list-style-type: none"> ➤ Staff recommends moving forward with Option 2 for the grant application ➤ Work with MnDOT to identify additional funding if not successful with federal grant



PROJECT FINANCING/FUNDING

IMPROVEMENT TYPE	AMOUNT
Roadway	\$5,950,000
Storm Sewer	\$1,400,000
Watermain	---
Retaining Walls	\$1,500,000
Landscaping	\$350,000
Eng./Admin. (20%)	1,800,000
TOTAL	\$11,000,000

FUNDING SOURCE	AMOUNT
MnDOT STIP	\$2,500,000 ¹
Federal STP	\$7,000,000 ²
City Funds	\$1,500,000 ³
TOTAL	\$11,000,000

¹Approximate contribution from proposed MnDOT pavement rehab project (FY 2020). Actual funding level could increase

²\$7,000,000 is the maximum amount available through the Met Council Regional Solicitation process

³A minimum of \$1,500,000 is required to provide local match to federal funds. Additional funding sources could reduce this amount



SCHEDULE

- ▶ Council Work Session – June 27, 2016
- ▶ City Council Action – July 5, 2016
 - ▶ Resolution supporting the federal funding application
- ▶ Federal STP Application Due – July 15, 2016
- ▶ Federal STP Funding Awarded – January/February 2016
- ▶ Begin Preliminary Engineering Studies – Spring 2016
 - ▶ If successful in receiving federal funding
- ▶ Design/Bidding Phase – 2016-2020
- ▶ Begin Construction 2020/2021



QUESTIONS??

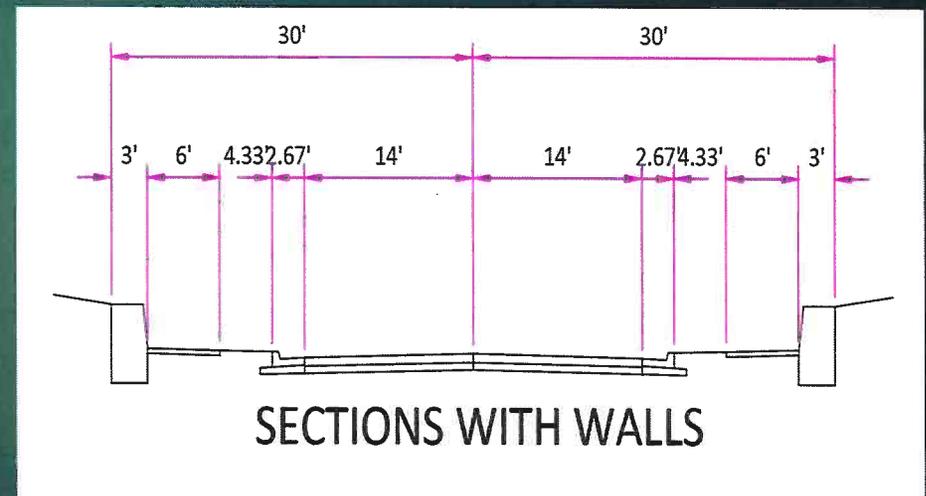
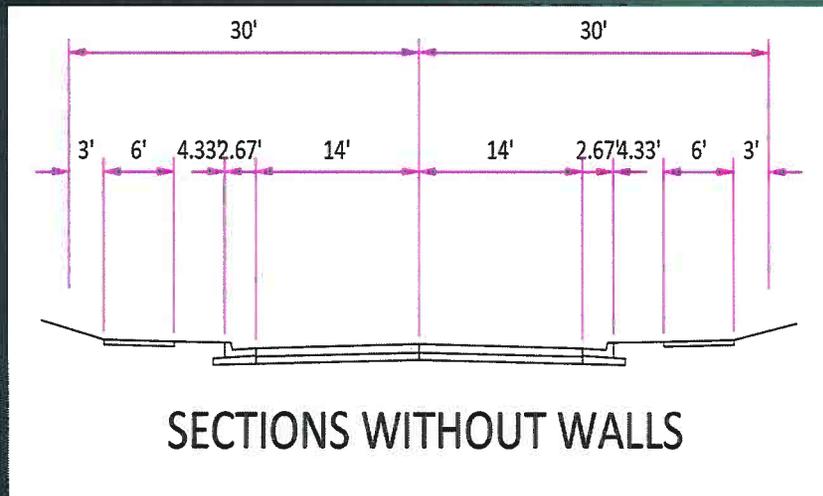
WENTWORTH AVENUE SIDEWALK IMPROVEMENTS



COUNCIL WORK SESSION
JUNE 27, 2016

WENTWORTH AVENUE SIDEWALK IMPROVEMENTS

- ▶ Summary of Possible Typical Sections
 - ▶ Sidewalk on South Side
 - ▶ Sidewalk on North Side
 - ▶ Sidewalk on Both Sides
 - ▶ Section west of 21st Ave. with parking bay, slightly different

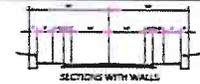
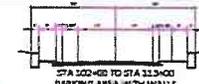
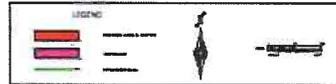


WENTWORTH AVENUE SIDEWALK IMPROVEMENTS

Preliminary Layout



CITY OF SOUTH ST. PAUL
WENTWORTH AVE (TH 52 TO 15TH AVE)
PRELIMINARY DESIGN JUNE 14, 2016



	South Side Sidewalk	North Side Sidewalk	Sidewalk on Both Sides
Pros	<ul style="list-style-type: none"> • Lower Costs (\$457,000) • No Permanent Esmts • Fewer walls (570 sf) • Fewer trees and Driveways • Will score better due to cost effectiveness 	<ul style="list-style-type: none"> • Fewer street crossings • No utility poles 	<ul style="list-style-type: none"> • Creates the most accessibility • If funded, takes fullest advantage of federal funding
Cons	<ul style="list-style-type: none"> • More street crossings • Utility pole moves 	<ul style="list-style-type: none"> • More trees and driveways • More walls to build (2100 sf) • More property impacts • More expensive (\$702,000) • Will require Permanent Esmts • Have to deal with parking bay 	<ul style="list-style-type: none"> • Most expensive (\$1,142,000) • All of Cons of other 2 options

	South Side Sidewalk	North Side Sidewalk	Sidewalk on Both Sides
Recommendation	<ul style="list-style-type: none"> ➤ If sidewalk only on one side this is the recommended side. ➤ City can perform construction inspection to reduce City out-of-pocket costs (all options) 	<ul style="list-style-type: none"> ➤ If sidewalk only on one side this is <u>NOT</u> recommended side 	

➤ **Staff Recommendation Considerations**

- Consider what is the right thing to do regardless of federal funding opportunities
- All options will need to consider temporary easements and potentially the use of eminent domain
- What is the City willing to support given the impacts to properties

PROJECT FINANCING/FUNDING

Options	Construction Costs (Federal Funding Eligible)	Engineering (25%) and Easement Costs	Total Project Costs	City Share (amount that can be offset if City does inspections)	County Share
South Side	\$350,000	\$107,000	\$457,000	\$48,000 (\$19,000)	\$59,000
North Side	\$515,000	\$188,000	\$703,000	\$85,000 (\$28,000)	\$103,000
Both Sides	\$850,000	\$292,000	\$1,142,000	\$131,000 (\$47,000)	\$161,000

Questions?